

4'-10 1/16"
4'-11 1/16"
4'-11 1/16"
4'-11 5/8"
4'-11 1/2"
5'-0 1/16"
4'-0 1/8"
4'-2 1/2"
4'-3 1/4"
4'-3 1/16"
4'-5 1/16"
4'-7 1/16"

2'-11 3/8"
3'-0 1/16"
3'-0"
2'-11 15/16"
2'-11 7/8"
3'-0 5/16"
2'-7 1/16"
2'-8 3/8"
2'-8 13/16"
2'-9 5/16"
2'-11"
3'-0 5/8"

1'-7 7/8" S1
1'-7 1/2" S2
1'-7 7/8" S3
1'-7 7/8" S4
1'-7 3/8" S5
1'-7 7/16" S6
1'-6 1/16" S7
1'-6 5/16" S8
1'-6 1/16" S9
1'-6 13/16" S10
1'-8 5/16" S11
1'-9 3/4" S12

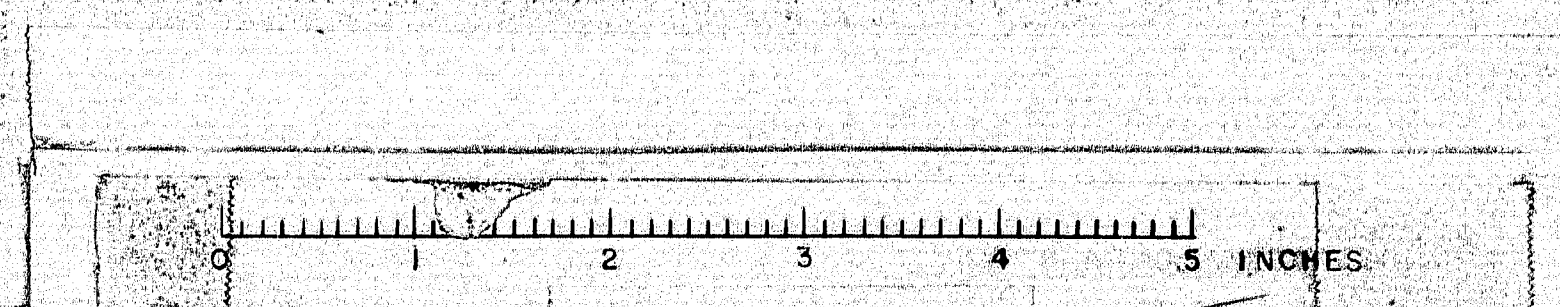
SHOP CONNECTIONS:
FIELD CONNECTIONS:
HOLES:
PAINT:

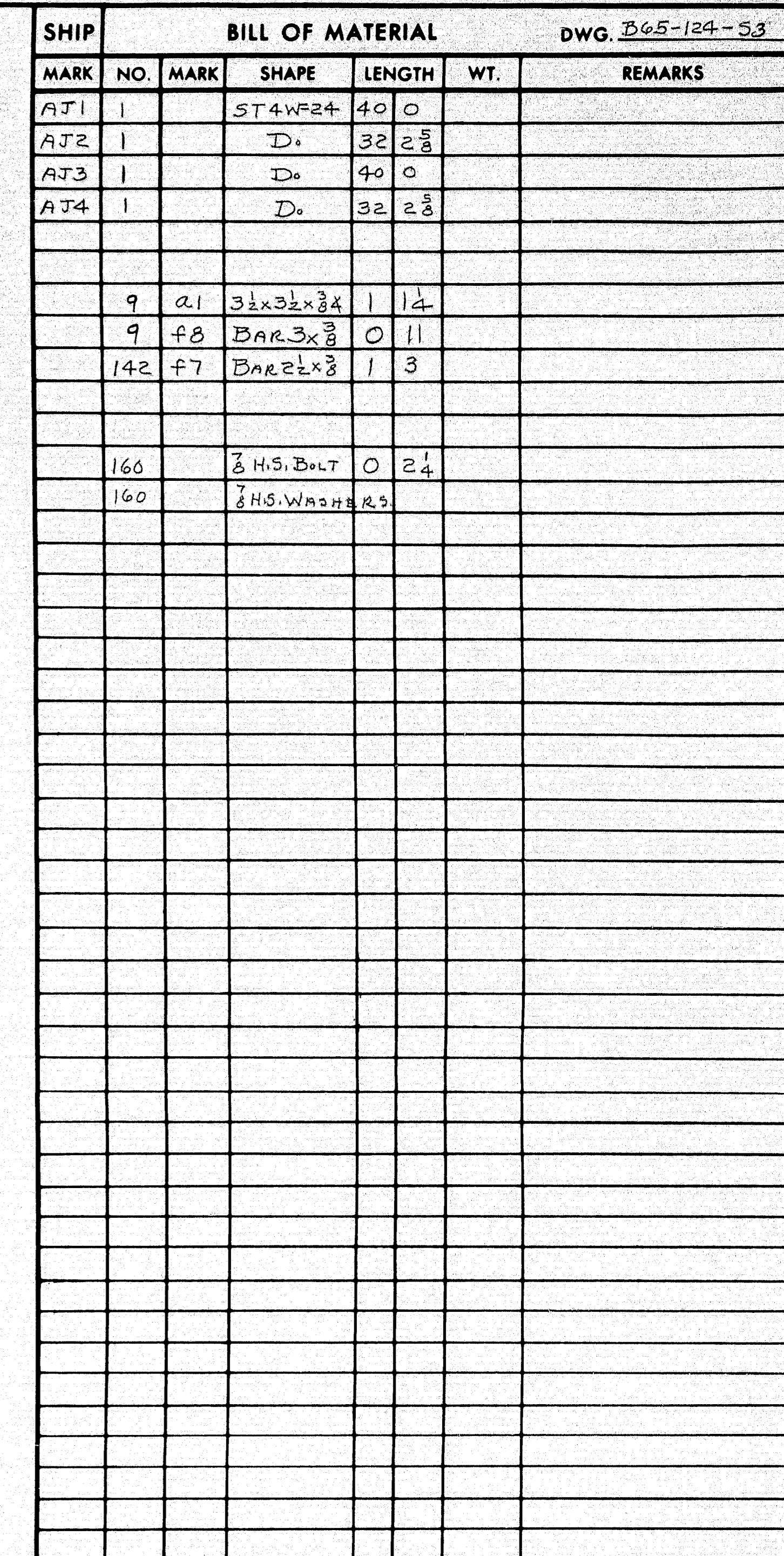
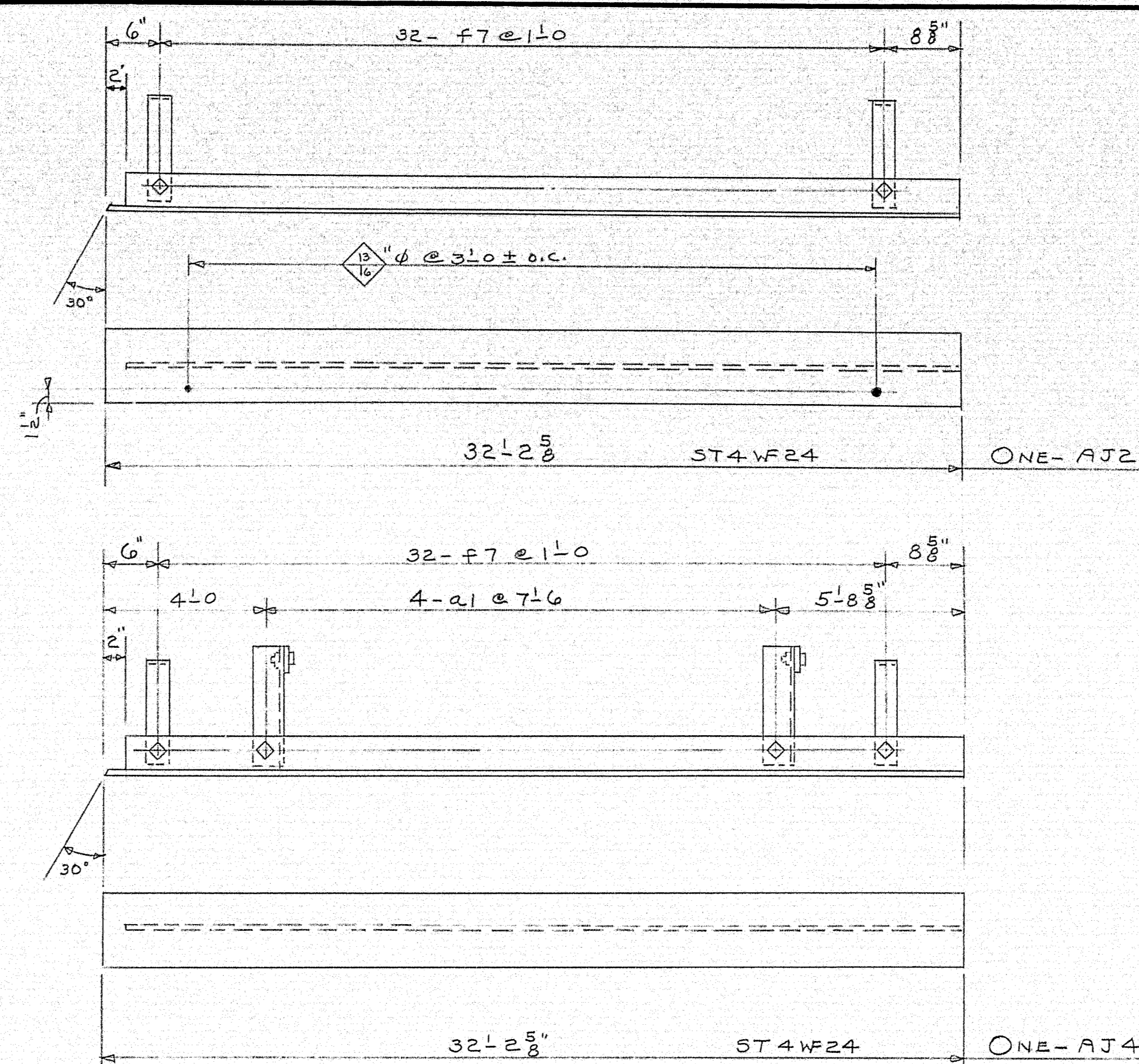
PROJECT No. I-95-9 (14)

DIAGRAM OF STRINGER ELEVATIONS

PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
3	SHOP 9-3-65	195 OVER U.S. RTE 2 - N.B. & S.B. SMYRNA MAINE	
8	DIST. 9-3-65		
2	F.A. 7-22-65		
2	F.A. 7-8-65		
DRAWN	7-6-65 D.C.	CUSTOMER REED & REED	
REVISION		DESIGNER STATE HIGHWAY COMM.	
REVISION		ORDER VERBAL DWG. B65-124- E3	
REVISION			

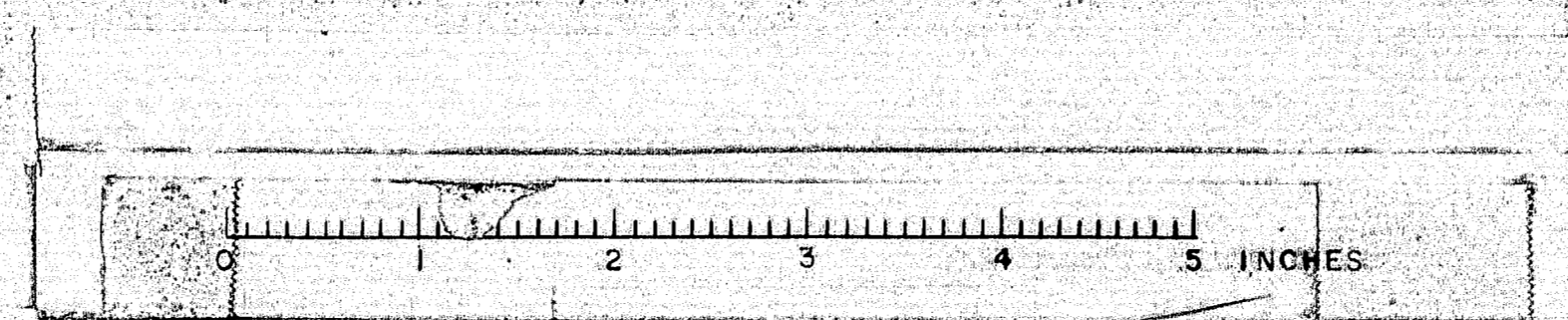
97-196B

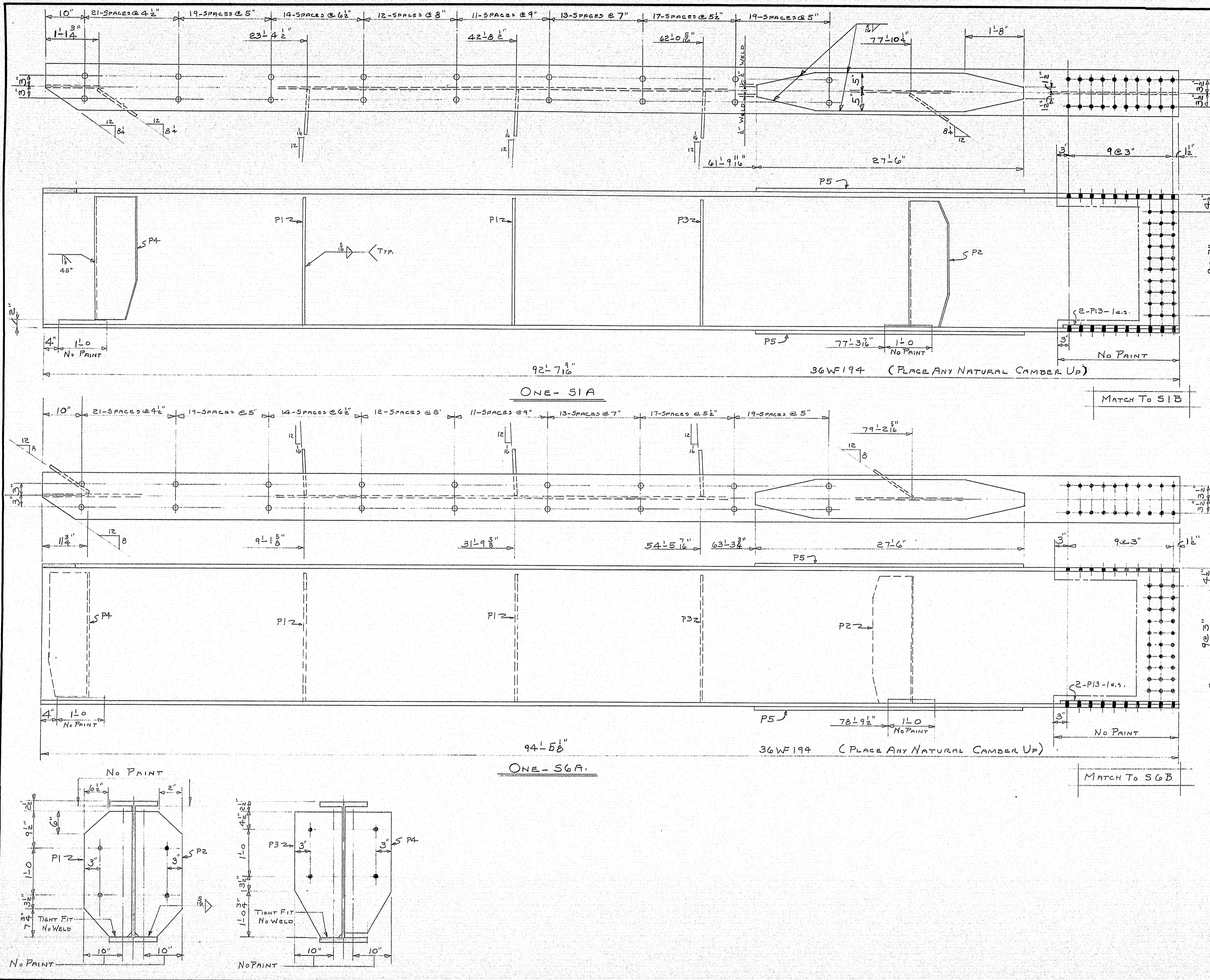




PRINT ISSUE		<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>
8 DIST.	9-3-65	
5 SHOP	9-1-65	
2 F.P.A.	7-22-65	
DRAWN	7-12-65 D.C.	I 95 OVER US RTE. 2 SOUTHBOUND SMYRNA MAINE CUSTOMER <u>REED & REED</u> DESIGNER <u>STATE HIGHWAY COMM.</u> ORDER <u>VERBAL</u> DWG. <u>B65-124-33</u>
REVISION		
REVISION		
REVISION		

97-196E



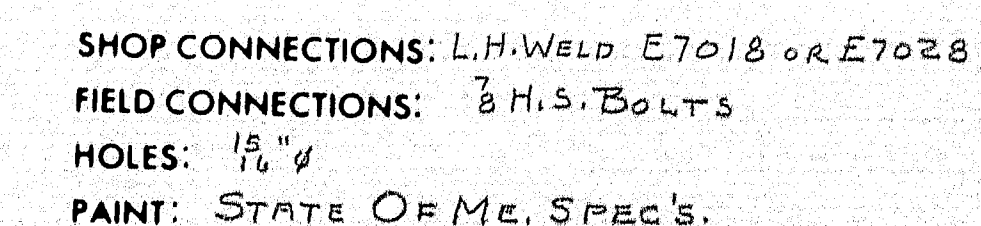
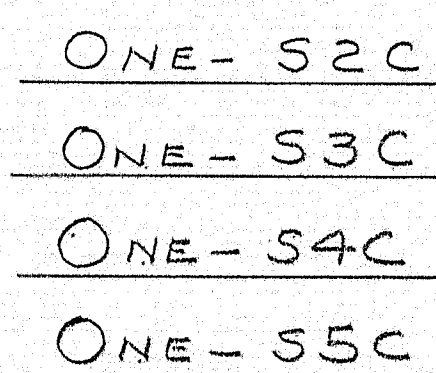


SHIP		BILL OF MATERIAL				DWG. B65-124-54
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
S1A	1		36WF194	92' 7 1/2"		
S6A	1		D _o	94' 5 1/2"		
	4	P1	PL 12 x 1/2	2' 8 1/4"		
	2	P2	D _o	2' 8 1/4"		
	2	P3	D _o	2' 8 1/4"		
	2	P4	D _o	2' 7"		
	4	P5	PL 10 x 1/4	27' 6"		
	4	P13	PL 4 x 5/8	2' 6"		
	508	3 STUDS	O 5			NELSON SHANK CONN'S.

SHOP CONNECTIONS: L.H. WELD E7028 OR E7018
 FIELD CONNECTIONS: 3/8 H.S. BOLTS
 HOLES: 1 1/2" Ø
 PAINT: STATE OF ME. SPEC'S.

PROJECT I-95-9(14)			
STRINGERS SPAN A		SOUTHBOUND	
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
5	SHOP	9-3-65	I 95 OVER U.S. RTE 2 SOUTHBOUND
8	DIST	9-3-65	SMYRNA MAINE
2	F.A.	7-22-65	CUSTOMER REED & REED
DRAWN	7-13-65	P.C.	DESIGNER STATE HIGHWAY COMM.
REVISION			ORDER VERDAL
REVISION			DWG. B65-124-54

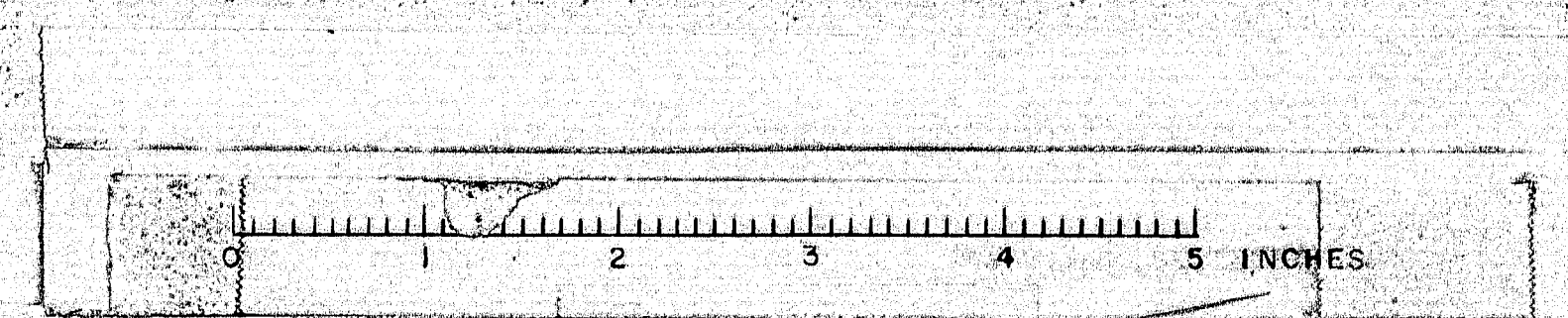
97-196F

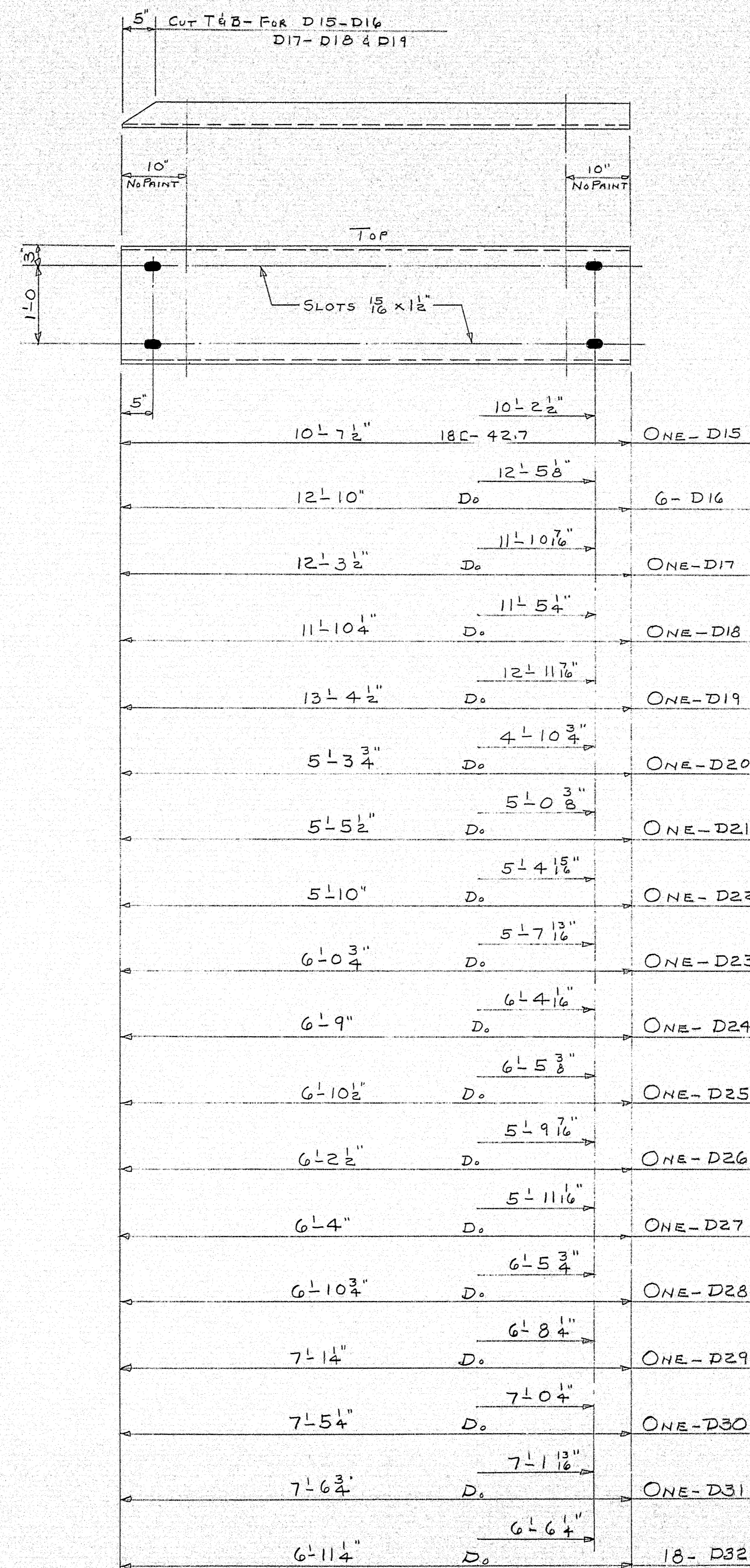


PROJECT I-95-9(14)

STRINGERS	SPAN C	SOUTHBOUND
PRINT ISSUE		
5 SHOP	9-3-65	<i>Pancroft & Martin Inc.</i> <i>Brewer, Maine</i> I 95 OVER U.S. RTE #2 SOUTHBOUND SMYRNA MAINE
8 DIST.	9-3-65	
2 F.I.R.	7-22-65	
DRAWN	7-14-65 D.C.I.	
REVISION		CUSTOMER <u>REED & REED</u> DESIGNER <u>STATE HIGHWAY COMM.</u> ORDER <u>VERBAL</u> DWG. <u>B65-124-S7</u>
REVISION		
REVISION		

97-196 I

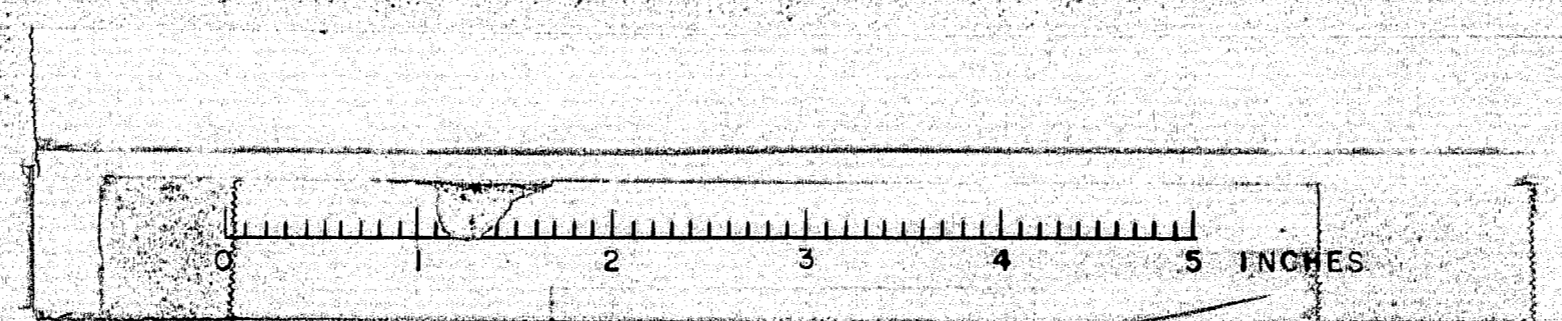


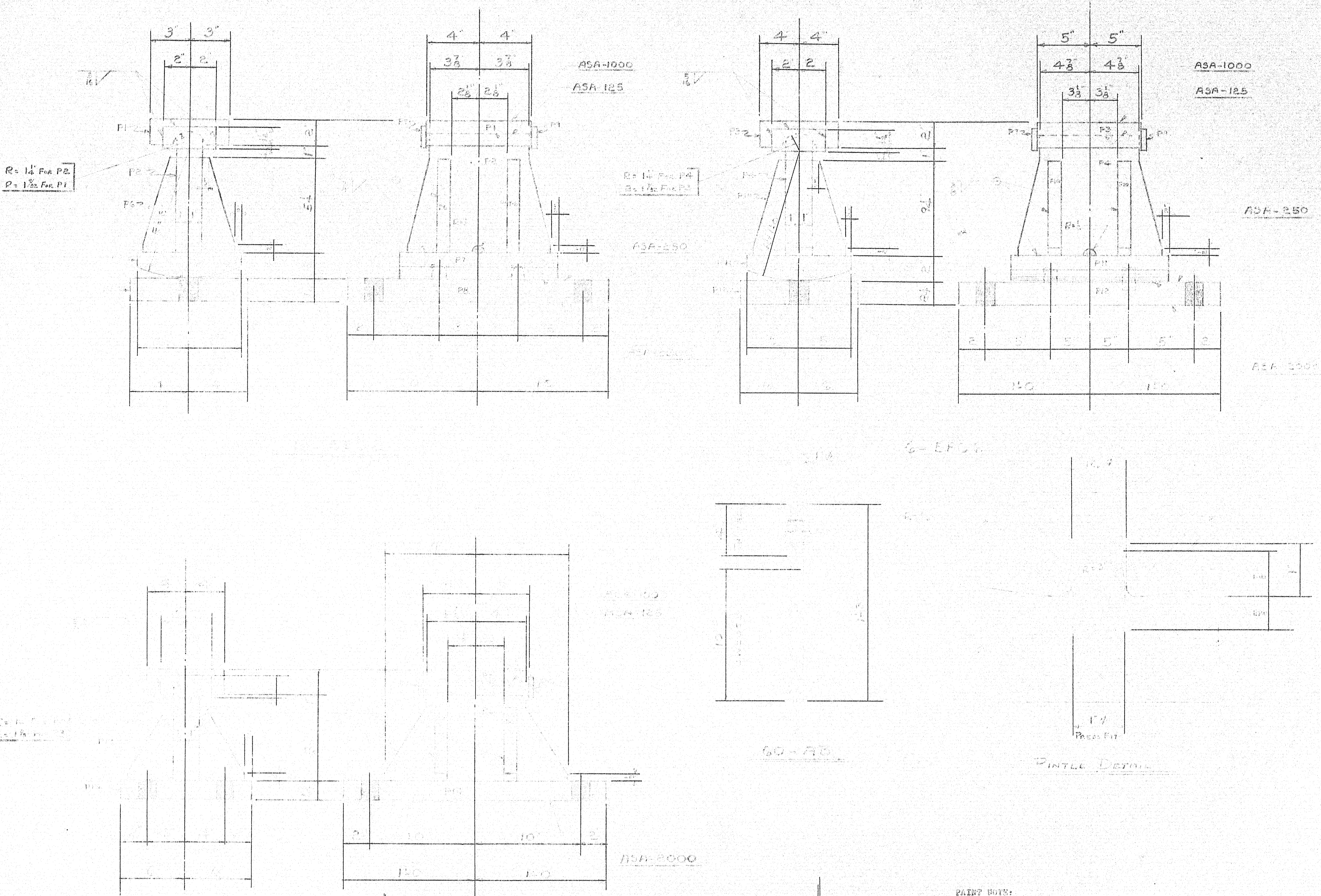


SHOP CONNECTIONS:
FIELD CONNECTIONS: 3/4" M. BOLTS & WELD
HOLES: AS NOTED
PAINT: STATE OF ME. SPEC'S.

PRINT ISSUE		<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>	
5 SHOP	9-3-65	I 95 OVER U.S. RTE. 2 SOUTHBOUND,	
8 DIST.	9-3-65	SMYRNA MAINE	
2 F.I.	7-22-65	CUSTOMER <u>REED & REED.</u>	
DRAWN	7-22-65 D.C.I.	DESIGNER <u>STATE HIGHWAY COMM.</u>	
REVISION		ORDER <u>VERBAL</u>	
REVISION		DWG <u>B65-124-59i</u>	
REVISION			

97-196K





R=14' For P2
D=12' For P1

ASA-250

ASA-250

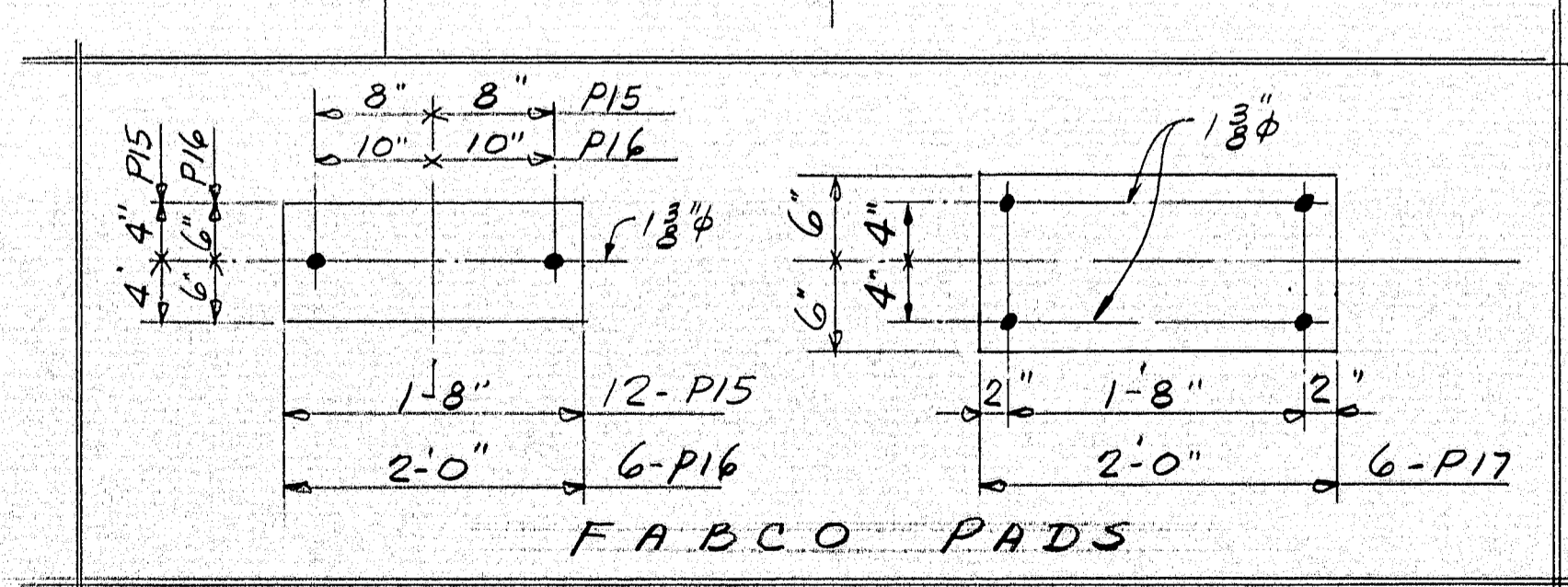
ASA-2000

6-EPG

60-AB

DETAIL DETAIL

ASA-2000



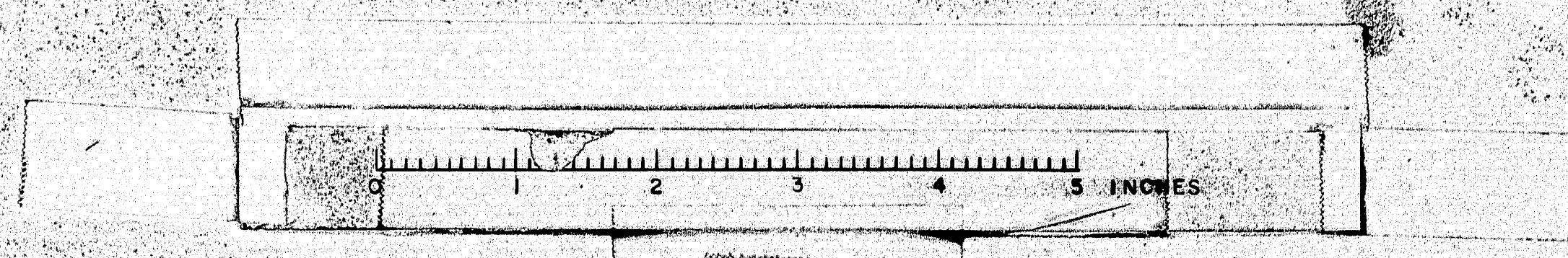
PAINT WORK:
No paint on anchor bolts - all threads
No paint on top surface & 1" down from top of sole plates - coat with boiled linseed oil.
No paint on surfaces finished ASA 250 - coat with hot mixture of white lead & tallow.
No paint on surfaces finished ASA 125

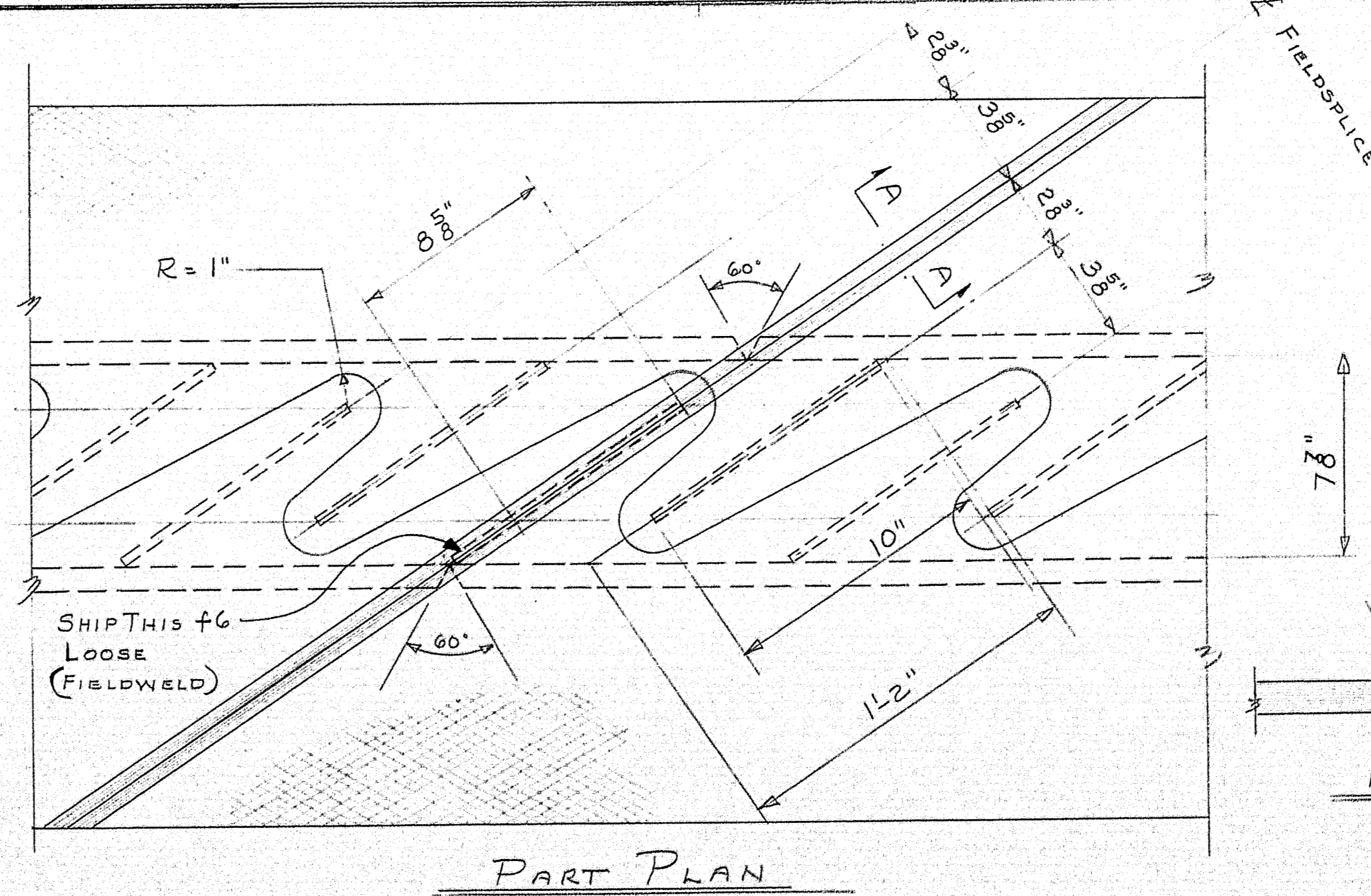
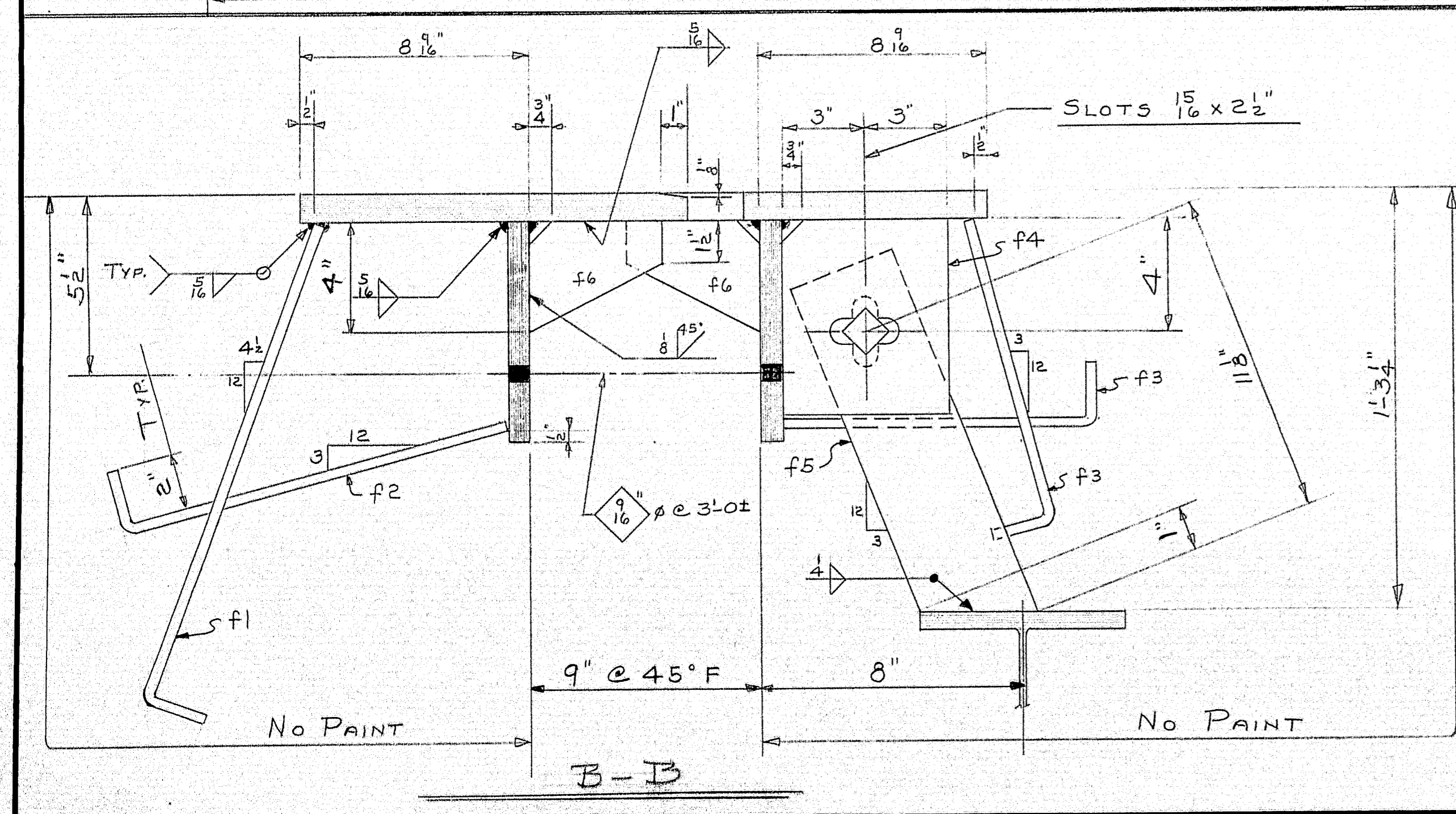
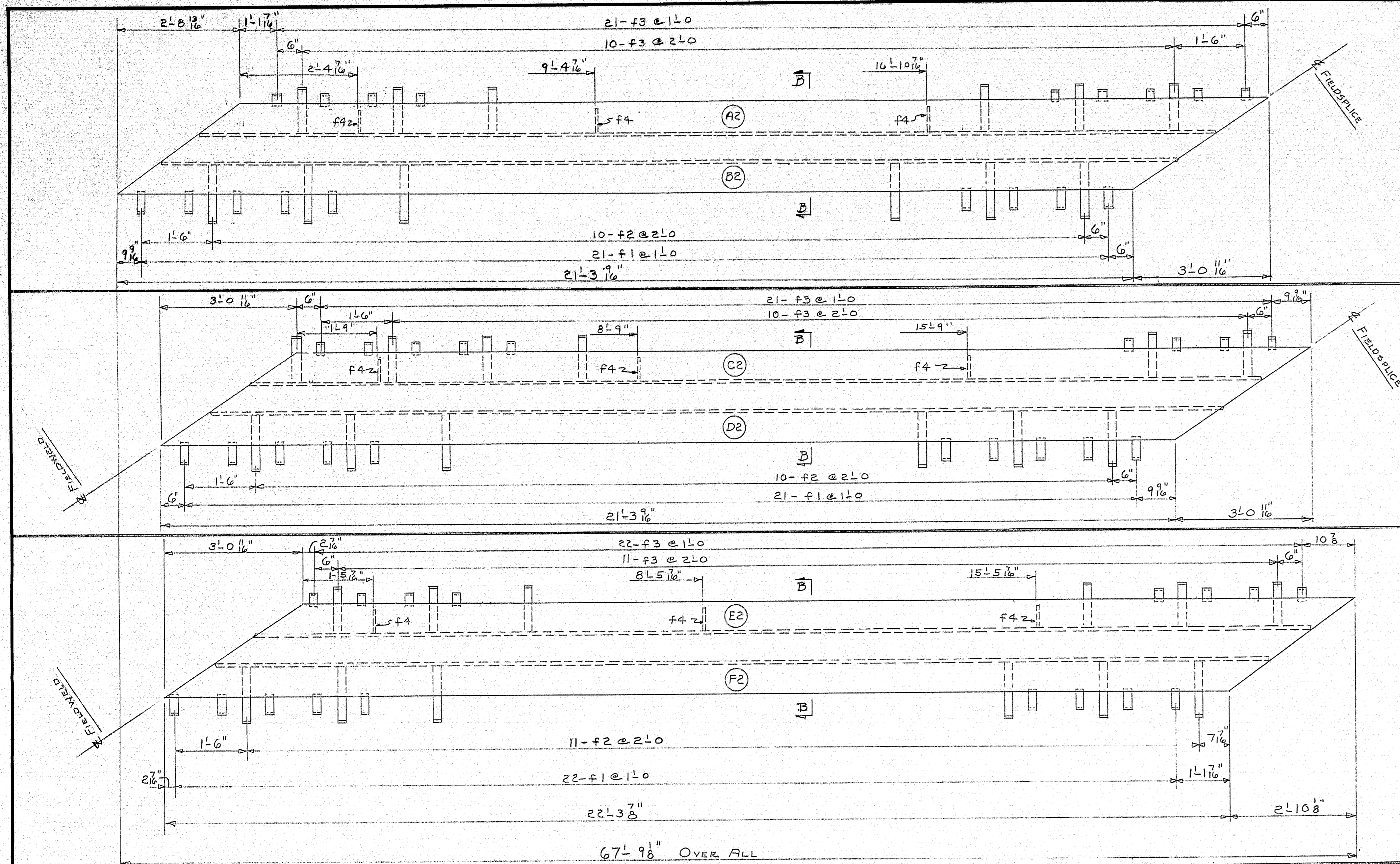
SHIP		BILL OF MATERIAL				DWG. B65-124-51
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
EPG-2	12		BRK. PEDESTALS			
EPG-6	6		D.			
EPG-4	6		D.			
	12	P1	R 6x2	0 8		
	12	P2	R 7x2	0 11		
	12	P3	R 8x2	0 10		
	6	P4	R 11x2	1 3		
	6	P5	R 7x3	1 6		
	48	P6	R 2x1	0 5 1/2		
	12	P7	R 7x2	1 0		
	12	P8	R 8x1 1/2	1 8		
	48	P9	R 12x3	0 4		
	24	P10	R 3 1/2 x 1	0 9 1/2		
	6	P11	R 10x2	1 4		
	6	P12	R 13x2	2 0		
	24	P13	R 4x1	0 5 1/2		
	6	P14	R 12x1 1/2	3 0		
	24	P15	1/8" R	0 12		
1E	60	1" DIA. NUTS				THREAD SCHEDULE
12		1" DIA. NUTS				
6		1" DIA. NUTS				
P15	12	8x8	1 8			"FABCO" PADS
P16	6	12x8	2 0			DO
P17	6	DO	2 0			DO

SHOP CONNECTIONS: WELD LHE E7018 OR E7028
FIELD CONNECTIONS: DO
HOLES: 1 3/8" UNLESS NOTED
PAINT: STATE OR M.E. SPEC'S. (SEE NOTE)

PROJECT I-95-9(14)				BEARING PEDESTALS (NORTHBOUND)	
PRINT ISSUE				Bancroft & Martin Inc. Brewster, Maine	
1 SHC. 9-14-65				I 95 OVER U.S. RTE. 2 NORTHBOUND	
8 DIST. 9-3-65				SILVERNA MAINE	
4 W/REQ. 7-23-65					
2 F.A. 7-8-65				CUSTOMER REED & REED	
DRAWN 7-8-65 D.C.				DESIGNER STATE HIGHWAY COMM.	
REVISION					
REVISION					
REVISION				ORDER VERBAL DWG. B65-124-510	

97-196L



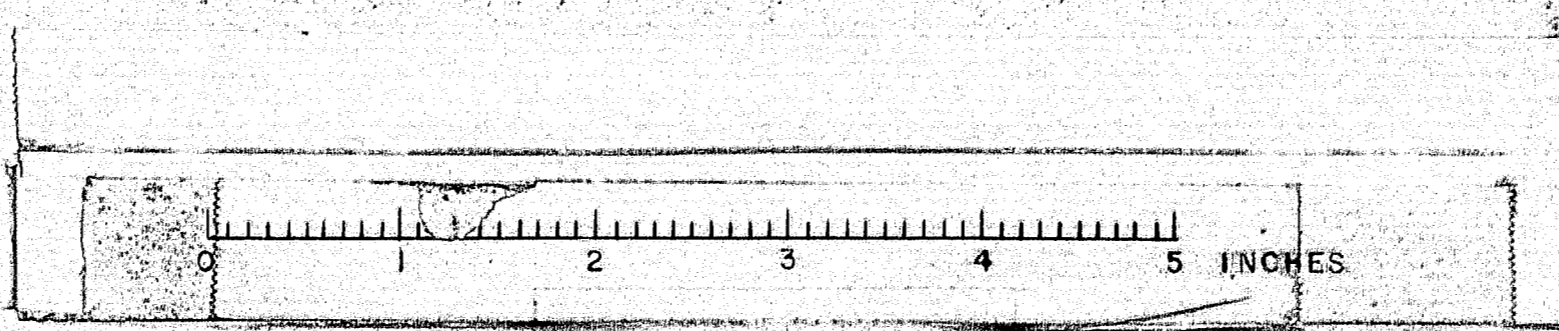


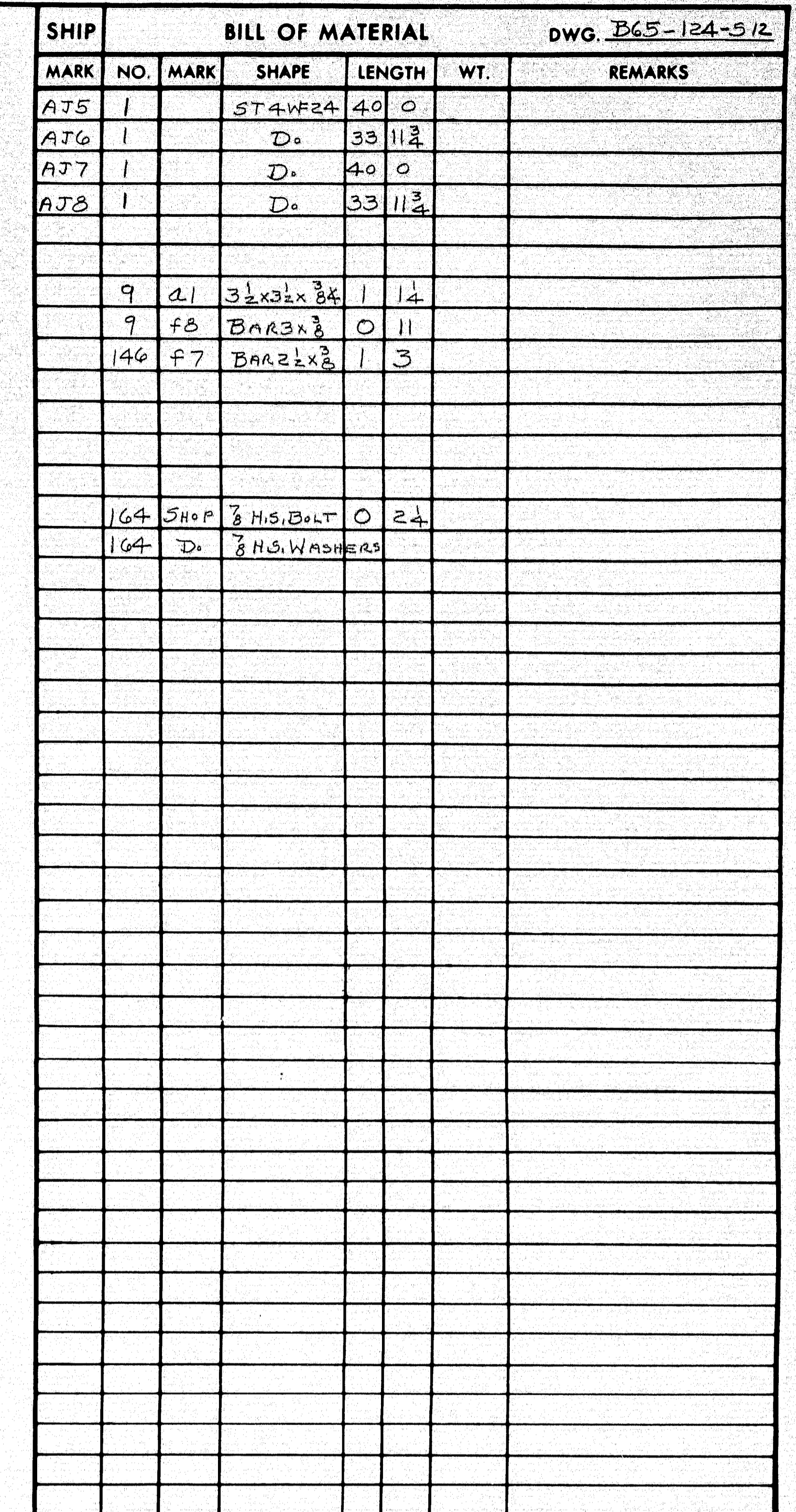
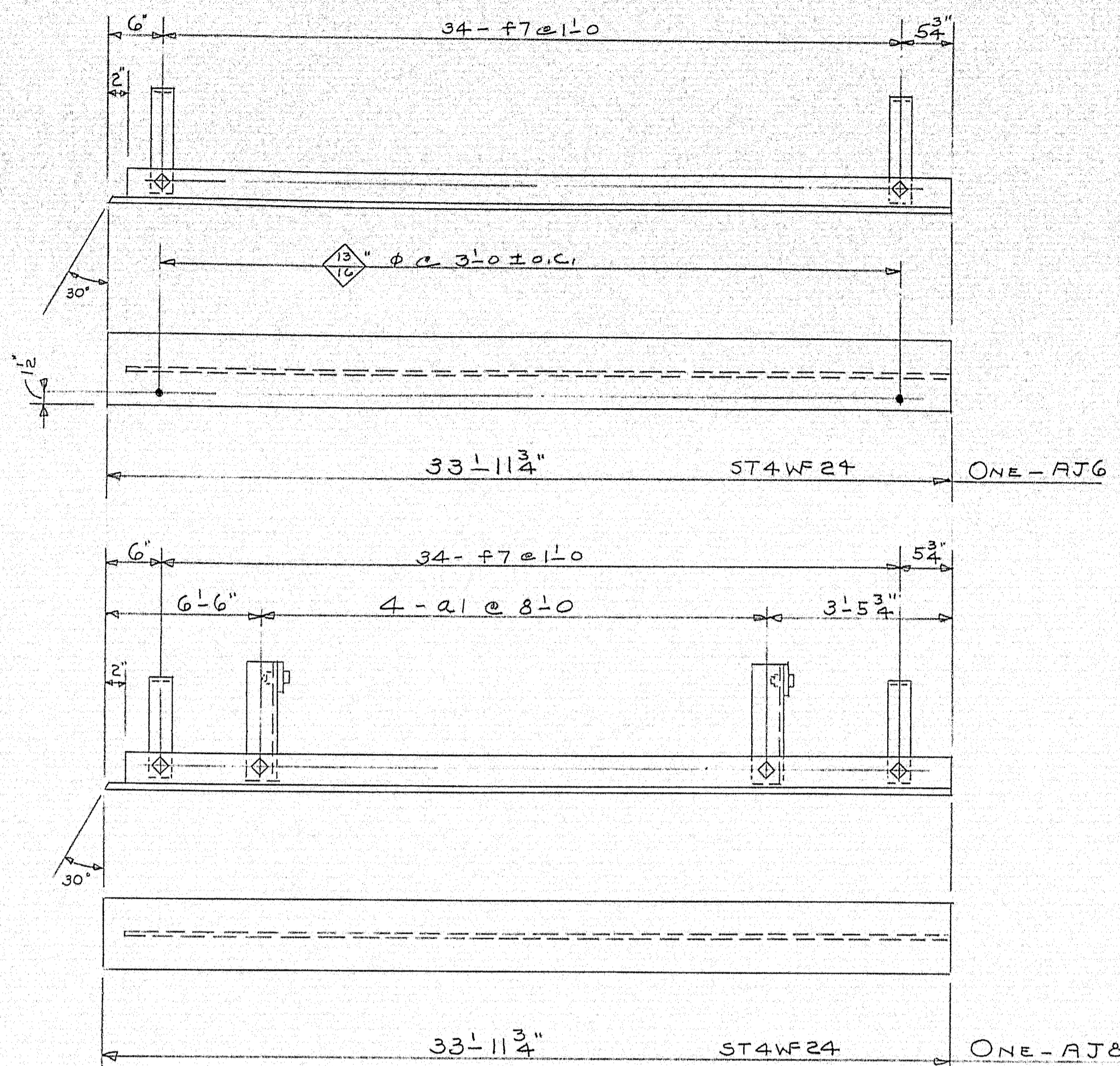
SHIP		BILL OF MATERIAL				DWG. D65-124-S11	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS	
A2	1		VNIT				
B2	1		D.				
C2	1		D.				
D2	1		D.				
E2	1		D.				
F2	1		D.				
	1	A4B	PE 25x1	24 4 1/2		CHK'D RE.	
	1	C4D	D.	24 4 1/2		D.	
	1	E4F	D.	25 2		D.	
	1	A	PE 25x3	21 6 1/2			
	1	B	D.	21 4 1/2			
	2	C4D	D.	21 3 1/2			
	1	E	D.	22 2 1/2			
	1	F	D.	22 3			
	64	F1	BAR 2x3	1 8			
	31	F2	D.	1 4			
	95	F3	D.	1 1			
	9	F4	BAR 6x3	0 7			
	9	F5	BAR 4x3	1 2			
	147	F6	D.	0 10		SHIP 2 LOOSE	
	9	SHOP	3/8 H.S. BOLT	0 2 1/2			
	9	D.	3/8 H.S. WASHERS				

SHOP CONNECTIONS: L.H. WELD- E7018 OR E7028
 FIELD CONNECTIONS: WELD
 HOLES: AS NOTED
 PAINT: STATE OF ME. SPEC'S.

PROJECT NO. I-95-9-(14)		EXPANSION DAM- ABUT. #1 (NORTHBOUND)	
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
8 DIST 9-3-65		195 OVER U.S. RTE #2 NORTHBOUND SMYRNA MAINE	
5 SHOP 9-1-65		CUSTOMER REED & REED	
2 F.A.I. 7-22-65		DESIGNER STATE HIGHWAY COMM.	
DRAWN 7-9-65 D.C.		ORDER VERBAL	
REVISION		DWG. B65-124-S11	
REVISION			

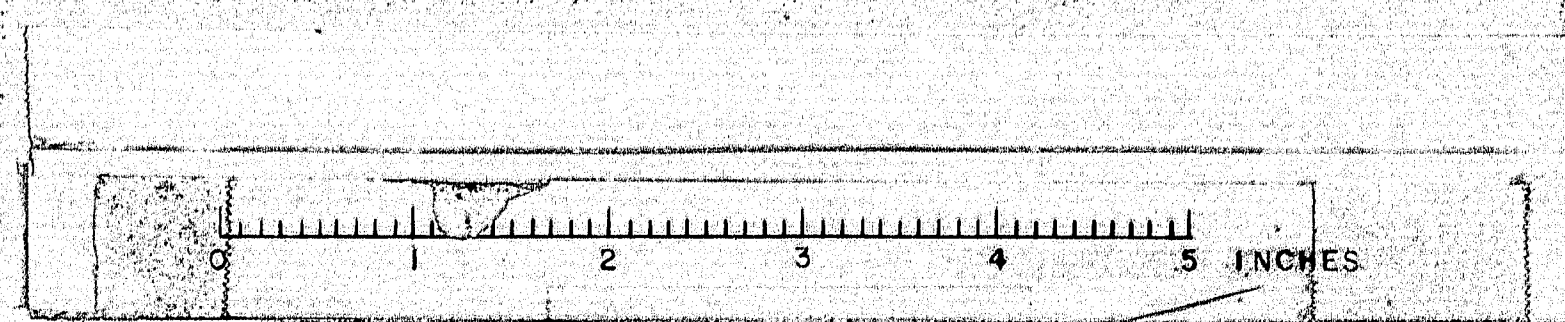
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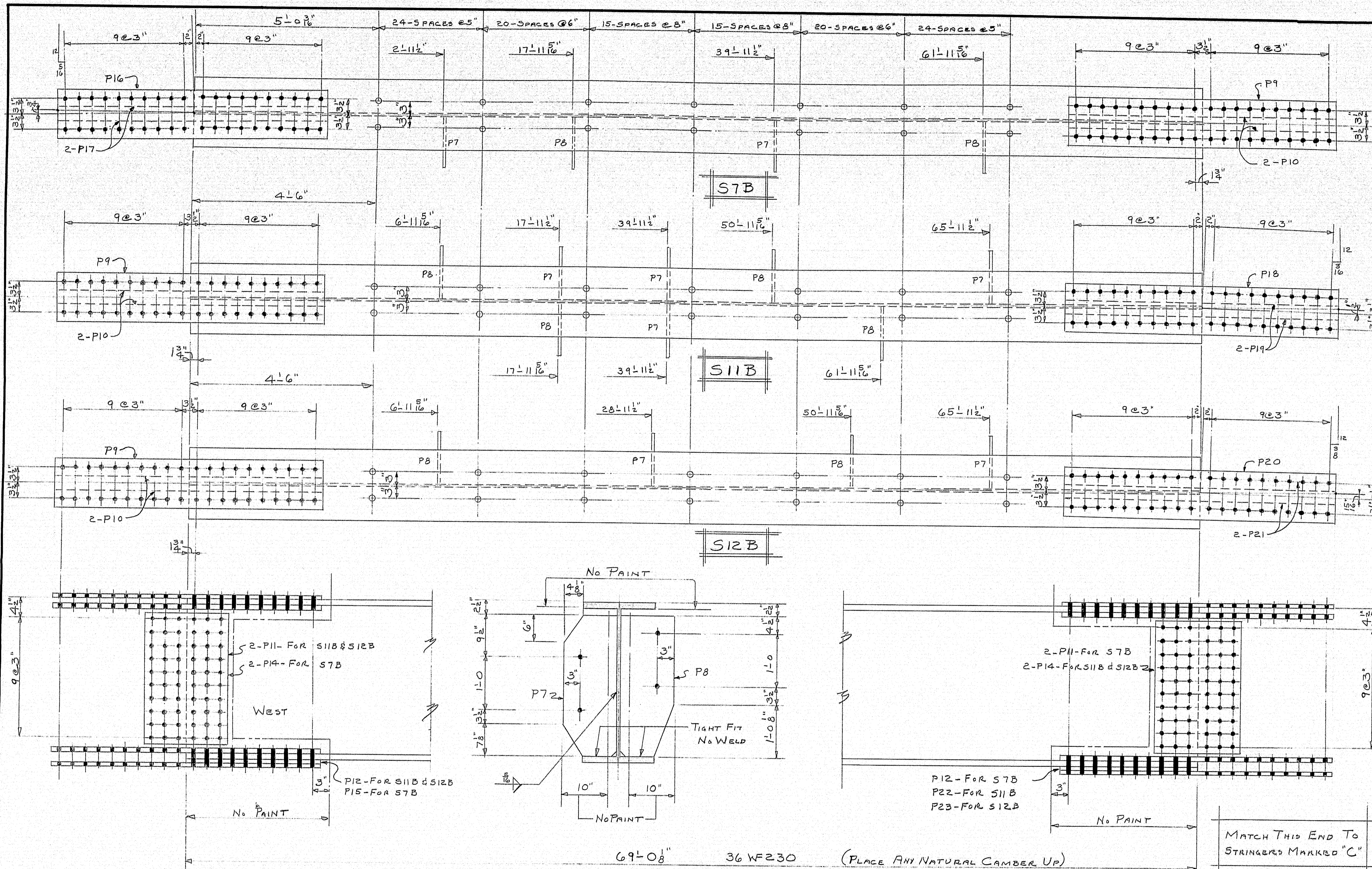




PRINT ISSUE			<i>Bancroft & Martin Inc.</i> <i>Brewer, Maine</i>	
8 24-15 APR 20	8	DIST	9-3-65	I95 OVER U.S. RTE.2 NORTHBOUND SMYRNA MAINE
	5	SHOP	9-1-65	
	2	FIA	7-22-65	
	DRAWN	7-12-65	D.C.	
	REVISION			
	REVISION			CUSTOMER <u>REED & REED</u> DESIGNER <u>STATE HIGHWAY COMM.</u>
	REVISION			
	ORDER	VERBAL	DWG. B65-124-512	

97-196N



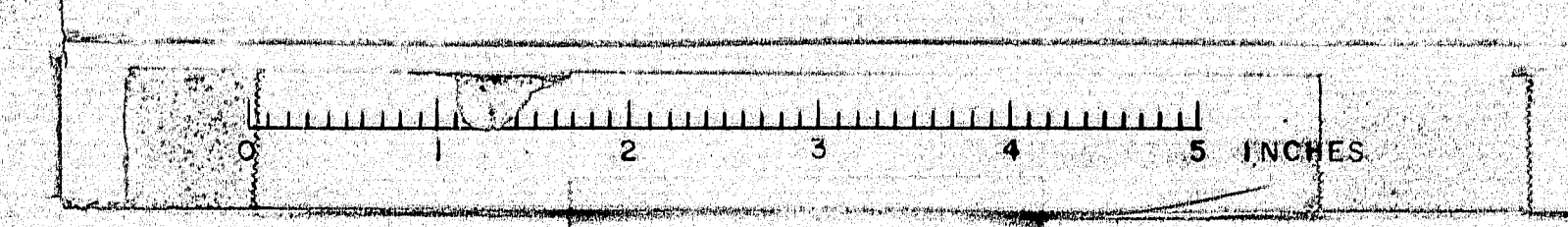


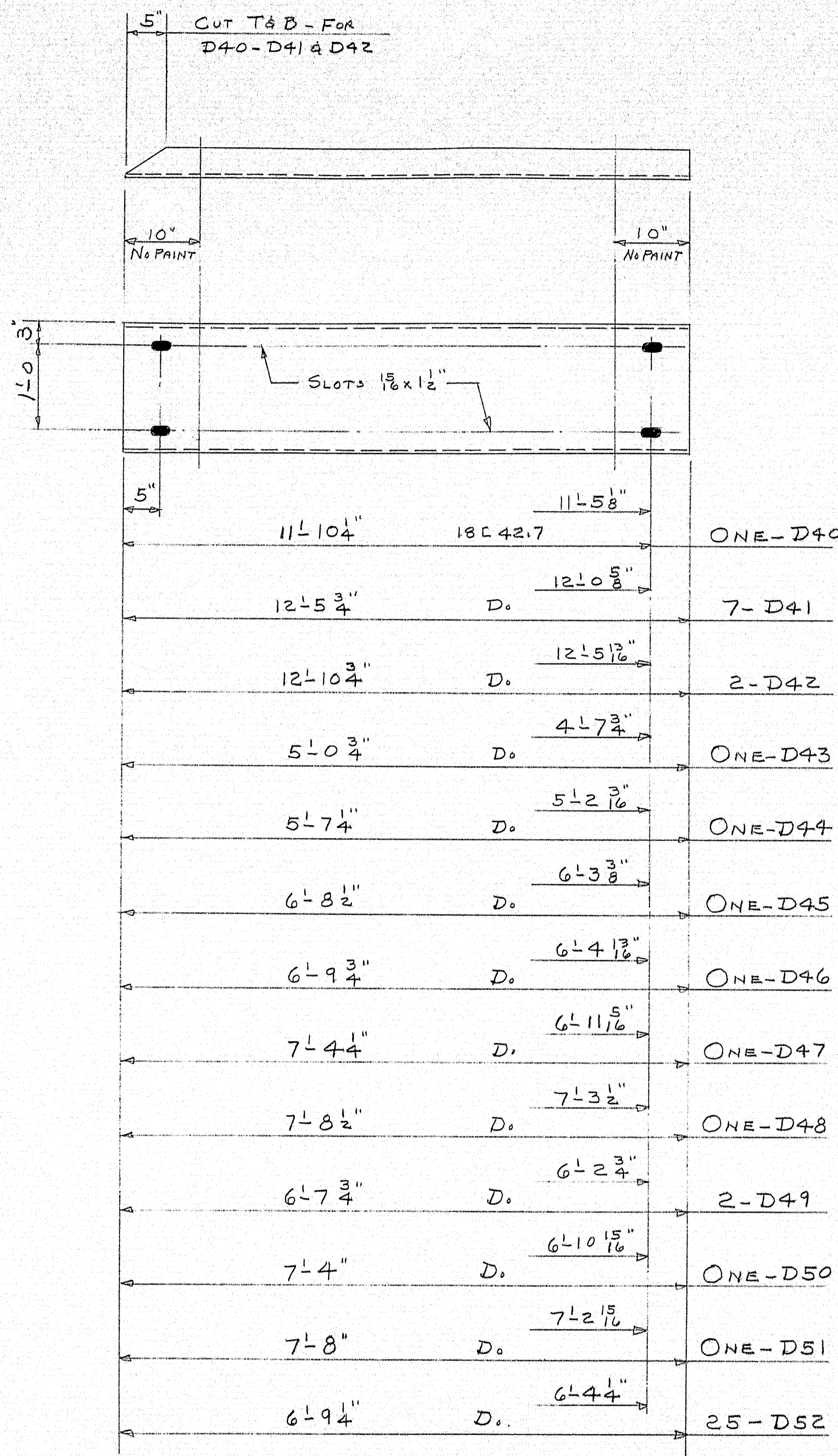
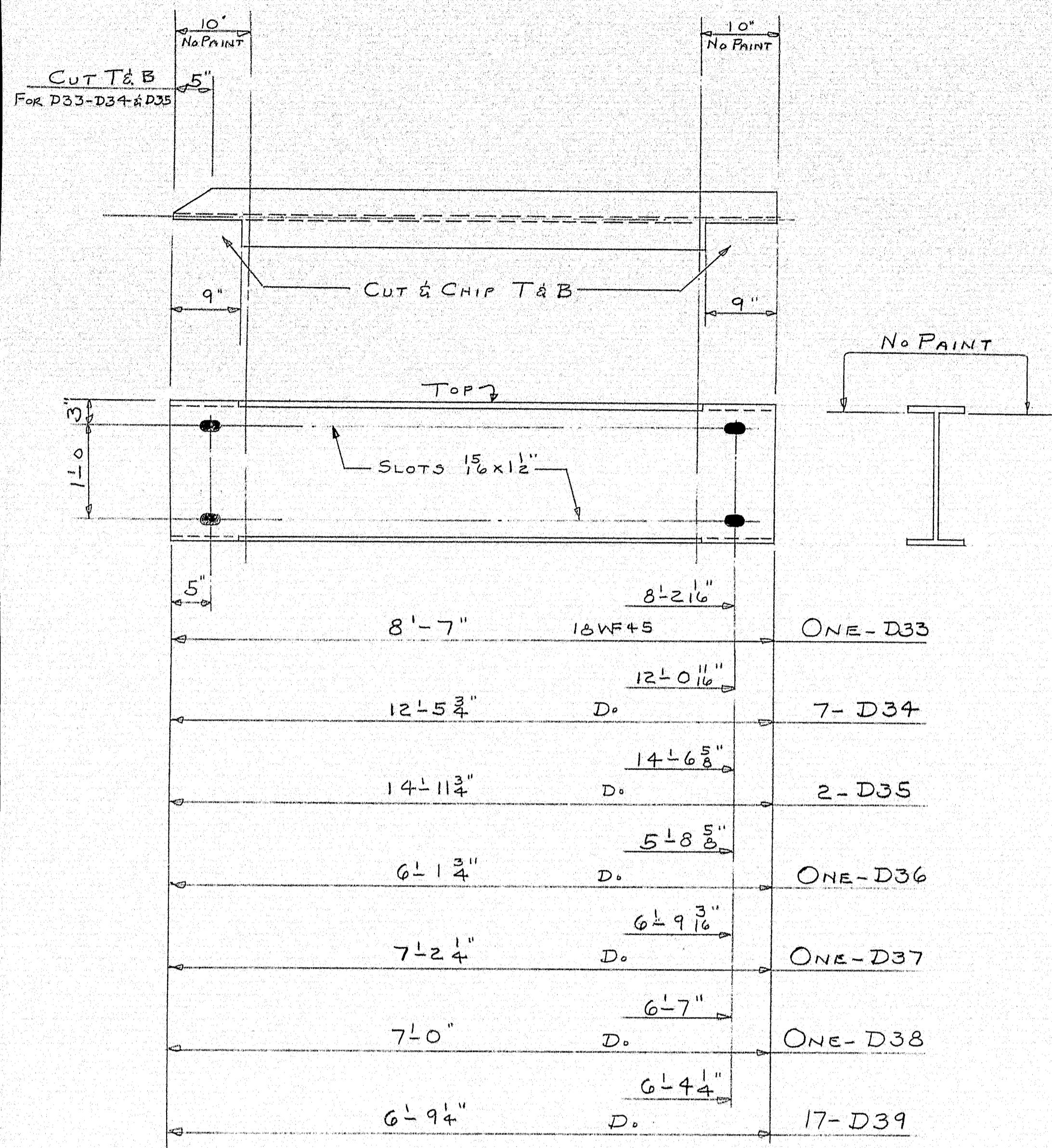
SHIP		BILL OF MATERIAL			DWG. B65-124-516	
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
S7B	1		36 WF230	69' 0 1/2"		
S11B	1		D.	69' 0 1/2"		
S12B	1		D.	69' 0 1/2"		
	8	P7	R 12 x 1/2	2' 8 1/2"		
	8	P8	D.	2' 8 1/2"		
	6	P9	R 11 x 3/4	5' 0 1/2"		
	2	P16	D.	5' 1"		
	2	P18	D.	5' 1"		
	2	P20	D.	5' 1"		
	12	P10	R 4 x 1	5' 0 1/2"		
	4	P17	D.	5' 1"		
	4	P19	D.	5' 1"		
	4	P21	D.	5' 1"		
	6	P11	R 18 1/2 x 5/8	2' 6"		
	6	P14	R 19 x 5/8	2' 6"		
	3	P12	R 11 x 3/8	2' 6"		
	1	P15	D.	2' 6"		
	1	P22	D.	2' 6"		
	1	P23	D.	2' 6"		

SHOP CONNECTIONS: L.H. WELD E7018 OR E7028
 FIELD CONNECTIONS: 3/4 HS. BOLTS
 HOLES: 1 5/16" Ø
 PAINT: STATE OF ME. SPEC'S.

PROJECT - I-95-9(14)	
STRINGERS SPAN B NORTHBOUND	
PRINT ISSUE	
5 SHOP	9-3-65
8 DIST	9-3-65
2 F.A.	7-23-65
DRAWN	7-21-65 D.C.
REVISION	
REVISION	
REVISION	
Bancroft & Martin Inc. Brewer, Maine	
I-95 OVER U.S. RTE #2 NORTHBOUND SMYRNA MAINE	
CUSTOMER REED & REED	
DESIGNER STATE HIGHWAY COMM.	
ORDER VERBAL	DWG. B65-124-516

97-196R



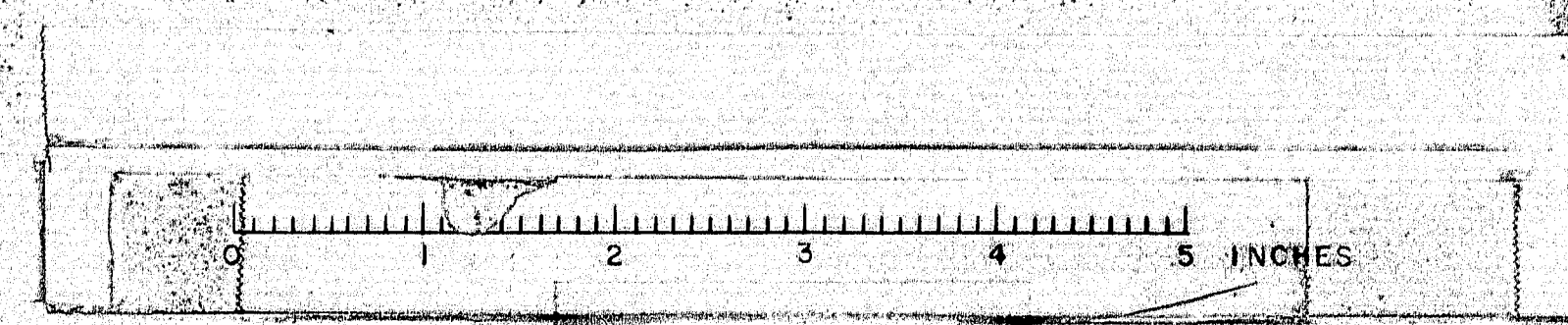


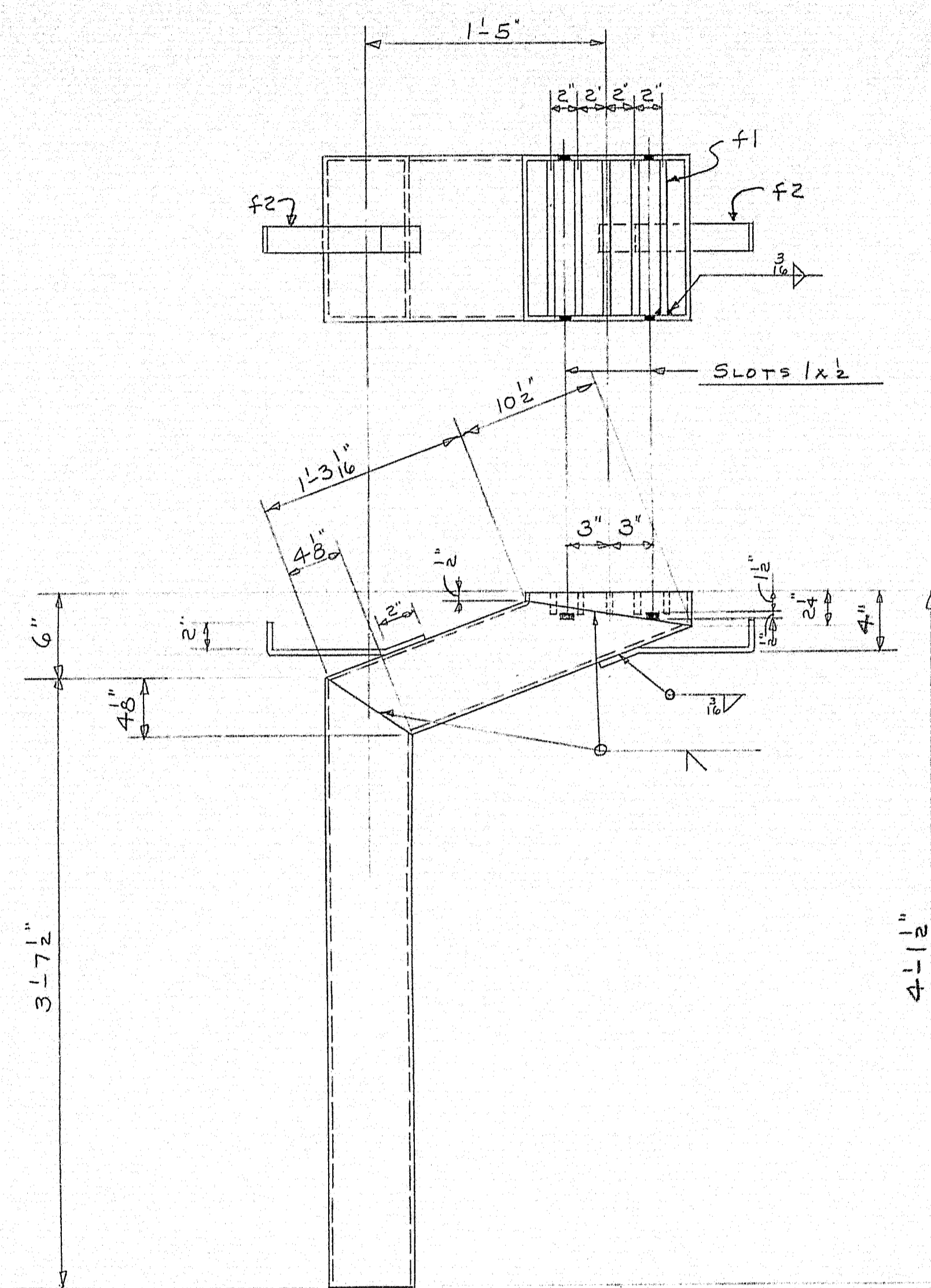
SHIP		BILL OF MATERIAL				DWG. B65-124-S19
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
D33	1		18WF45	8'7"		
D34	7		D.	12'5 3/4"		
D35	2		D.	14'11 3/4"		
D36	1		D.	6'1 3/4"		
D37	1		D.	7'2 1/4"		
D38	1		D.	7'0"		
D39	17		D.	6'9 1/4"		
D40	1		18C42.7	11'10 1/4"		
D41	7		D.	12'5 3/4"		
D42	2		D.	12'10 3/4"		
D43	1		D.	5'0 3/4"		
D44	1		D.	5'7 1/4"		
D45	1		D.	6'8 1/2"		
D46	1		D.	6'9 3/4"		
D47	1		D.	7'4 1/4"		
D48	1		D.	7'8 1/2"		
D49	2		D.	6'7 3/4"		
D50	1		D.	7'4"		
D51	1		D.	7'8"		
D52	25		D.	6'9 1/4"		

SHOP CONNECTIONS:
FIELD CONNECTIONS: 3/4" BOLTS & WELD
HOLES: AS NOTED
PAINT: STATE OF ME. SPEC'S.

PROJECT I-95-9 (14)		DIAPHRAGMS NORTHBOUND	
PRINT ISSUE		Bancroft & Martin Inc. Brewer, Maine	
5 SHIP	9-3-65	I 95 OVER U.S. RTE. 2 NORTHBOUND SMYRNA MAINE	
8 DIST.	9-3-65		
2 F.A.	7-22-65		
DRAWN	7-22-65 D.C.	CUSTOMER REED & REED	
REVISION		DESIGNER STATE HIGHWAY COMM.	
REVISION		ORDER VERBAL DWG. B65-124-S19	
REVISION			

97-196U





8 - DP

SHIP		BILL OF MATERIAL				DWG. B65-124-S20
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
DP1	8		12x6x5/16	3	7 1/2	STRUCTURAL TUBING
	8		D.	2	1 1/2	D.
	8		12x12x5/16	0	2 1/4	D.
	40	F1	BAR 1 1/2 x 1/2	0	11 5/8	
	16	F2	BAR 2 x 4	1	0	
DP2	8		ST6WF13.5	1	8"	N.FAB

SHOP CONNECTIONS: WELD
FIELD CONNECTIONS: WELD
HOLES:
PAINT: STATE OF ME. SPEC'S.

PROJECT I-95-9(14)		DRAINS. SOUTHBOUND & NORTHBOUND.	
PRINT ISSUE		Bancroft & Martin Rolling Mills Company Brewer, Maine	
5 SHC.	9-14-65	I 95 OVER U.S. RTE. 2 S.B. & N.B.	
5 SHOP	9-3-65	SMYRNA MAINE	
8 DIST.	9-3-65		
2 FAL	7-22-65	CUSTOMER REED & REED	
DRAWN	7-22-65 P.C.	DESIGNER STATE HIGHWAY COMM.	
REVISION		ORDER VERBAL	
REVISION		DWG. B65-124-S20	

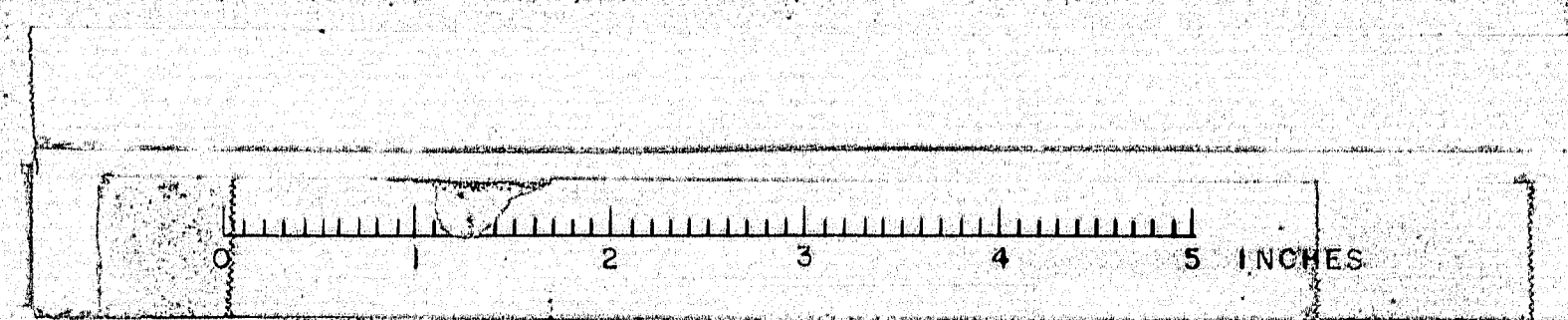
97-196V

SHIP		BILL OF MATERIAL				DWG. B65-124-S21
MARK	NO.	MARK	SHAPE	LENGTH	WT.	REMARKS
FIELD	1470		3/4 H.S. BOLT	0	3 1/2	SPLICE BOLTS
D.	980		D.	0	4 1/2	D.
D.	980		D.	0	5	D.
D.	3430		3/4 H.S. WASHERS			
D.	600		3/4 M. BOLT	0	2	FOR DIAPHRAGMS.

SHOP CONNECTIONS:
FIELD CONNECTIONS:
HOLES:
PAINT:

FIELD BOLTS FOR NORTHBOUND & SOUTHBOUND.		Bancroft & Martin Rolling Mills Company Brewer, Maine	
PRINT ISSUE		I 95 OVER U.S. RTE. 2 (N.B. & S.B.)	
		SMYRNA MAINE	
3 SHOP	10-18-65	CUSTOMER REED & REED	
DRAWN	10-18-65 D.C.	DESIGNER STATE HIGHWAY COMM.	
REVISION		ORDER VERBAL	
REVISION		DWG. B65-124-S21	

97-196W



ESTIMATE OF BRIDGE QUANTITIES

ITEMS	DESCRIPTION	S. B.		N. B.	
		UNITS	QUANTITY	QUANTITY	QUANTITY
204-12	Structural Earth Exc. Abutments and Ret. Wall	C. Y.	102		
204-14	Structural Earth Exc. - Piers	C. Y.	535	579	
701-33	P.C.C. Abut. & Retaining Wall	C. Y.	346	362	
701-35	P.C.C. Piers	C. Y.	233	293	
701-40	P.C.C. Roadway & Sidewalk Slabs on Steel Bridges	C. Y.	299	299	
701-55	Curing Box For Concrete Cylinders	Each	1		
702-103	Structural Steel Fabricated & Delivered	L. S.	Lump Sum	Lump Sum	
702-104	Structural Steel Erection	L. S.	Lump Sum	Lump Sum	
702-105	Structural Steel Field Painting	L. S.	Lump Sum	Lump Sum	
705-13	Reinforcing Steel - Delivered	Lbs.	163,900	174,700	
705-14	Reinforcing Steel - Placing	Lbs.	163,900	174,700	
705-17	Shear Connectors	L. S.	Lump Sum	Lump Sum	
705-16	Steel H-Beams Piles 42 lbs./ft.	L. F.	1,326	1,375	
805-8	Bridge Rail	L. F.	496	496	
807-11	Epoxy Resin Surface Sealant	S. Y.	215	219	
808-6	Slope Paving	S. Y.	897	907	
901-24	Vertical Bridge Curb Type 1	L. F.	504	504	
901-25	Vertical Bridge Curb Type 1 Circular	L. F.	16	16	

Estimated weight of structural steel including drains is 401,700 lbs. S. B. & 398,200 lbs. N. B. Total estimated weight = 799,900 lbs.

Estimated weight of Shear Connectors $\frac{5}{8}$ " spirals 10,240 lbs.
Estimated number of Shear Connectors $\frac{7}{8}$ " studs 8,712 Pcs.

INDEX OF DRAWINGS

- 1 Quantities
- 2 General Plan
- 3 Foundation Survey
- 4 Foundation Survey
- 5 Abutment No. 1
- 6 Abutment No. 2
- 7 Abutment No. 1
- 8 Abutment No. 2
- 9 Piers
- 10 Piers
- 11 Structural Steel & Blocking
- 12 Structural Steel & Blocking
- 13 Superstructure
- 14 Superstructure
- 15 Superstructure Details
- 16 Slope Paving
- 17 Reinforcing Steel
- 18 Reinforcing Steel

STANDARD DETAIL DRAWINGS

- BD-101-64--- Bearing Pedestals
BD-103-64--- Beam Splices
BD-104-64--- Diaphragms, Armored
Joint, Shear Connectors
Drains.
BD-105-64--- Expansion Dams
BD-107-64--- Steel Rail
BD-108-64--- Aluminum Rail

MAINE STATE HIGHWAY COMMISSION
AUGUSTA, MAINE

INTERSTATE 95
OVER
U.S. ROUTE 2
IN THE TOWN OF
SMYRNA
ARCUSTOOK COUNTY
QUANTITIES

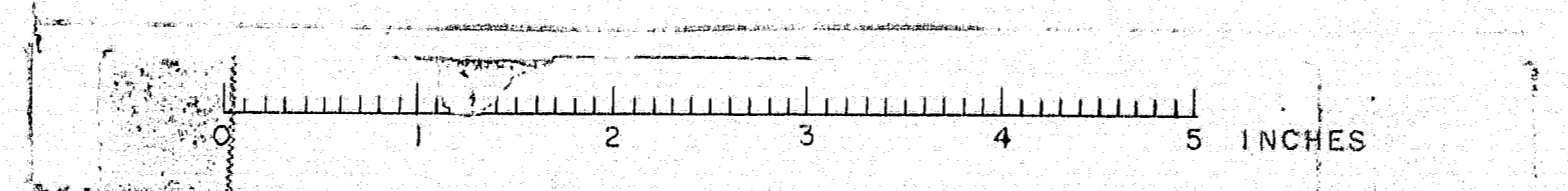
HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS

NEW YORK BOSTON KANSAS CITY

SHEET 1 OF 18

MARCH 1965
SMYRNA(14)

M-2222



S. P. R. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-9(14)	20	158

SPECIFICATIONS

DESIGN:

AASHTO Standard Specifications for Highway Bridges 1961 with Interim Specifications, 1961, 1962, 1963 & 1964.

CONTRACT:

State of Maine, State Highway Commission Standard Specifications for Highways and Bridges, Revision of January 1960 and Supplemental Specifications of Feb. 1960.

LIVE LOADING

HS 20-44 (Modified for Interstate)

FOUNDATIONS

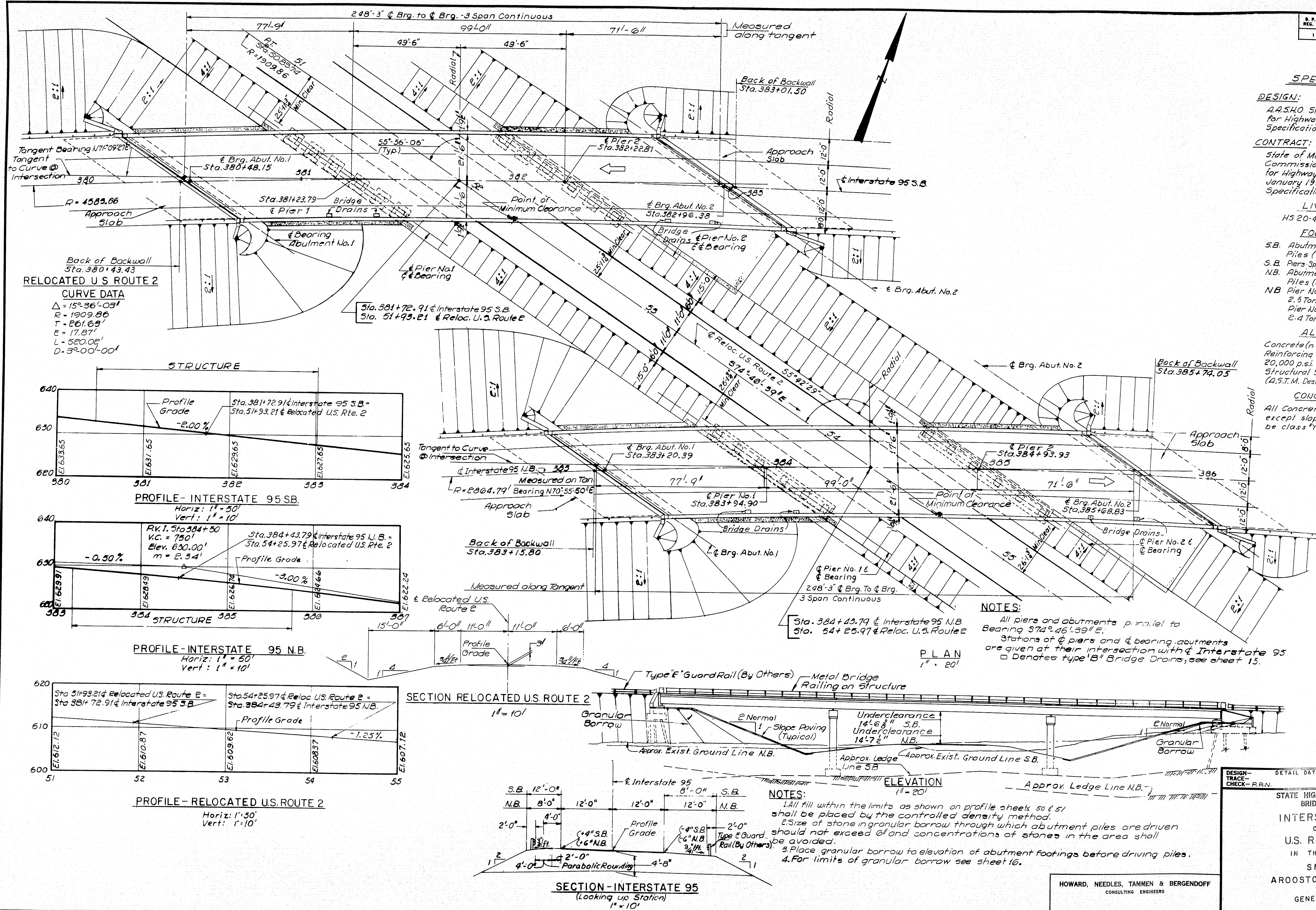
S.B. Abutments: 10 BP42 End Bearing Piles (37 Tons Capacity)
 S.B. Piers: Spread footings on ledge 4.4 tons/ft.
 N.B. Abutments: 10 BP42 End Bearing Piles (37 Tons Capacity)
 N.B. Pier No. 1: Spread footings on soil - 2.5 Tons/sq. ft.
 Pier No. 2: Spread footings on soil - 2.4 Tons/sq. ft.

ALLOWABLE STRESSES

Concrete (n=10) - $f_c = 1200$ p.s.i.
 Reinforcing Steel, Int. Grade - $f_s = 20,000$ p.s.i.
 Structural Steel - $f_s = 20,000$ p.s.i. (A.S.T.M. Designation A-36)

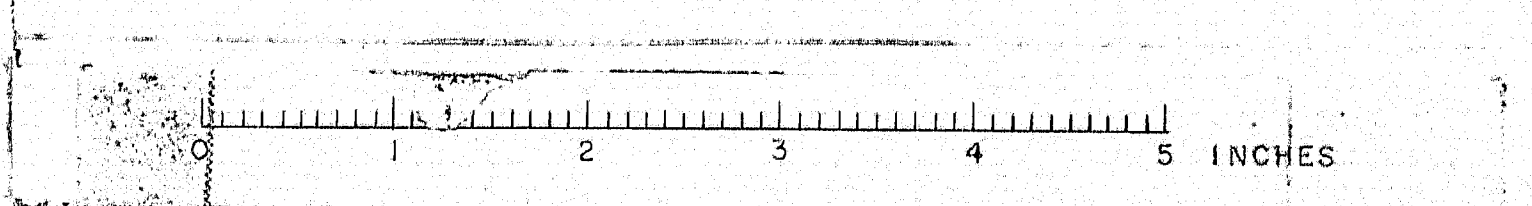
CONCRETE CLASSIFICATION

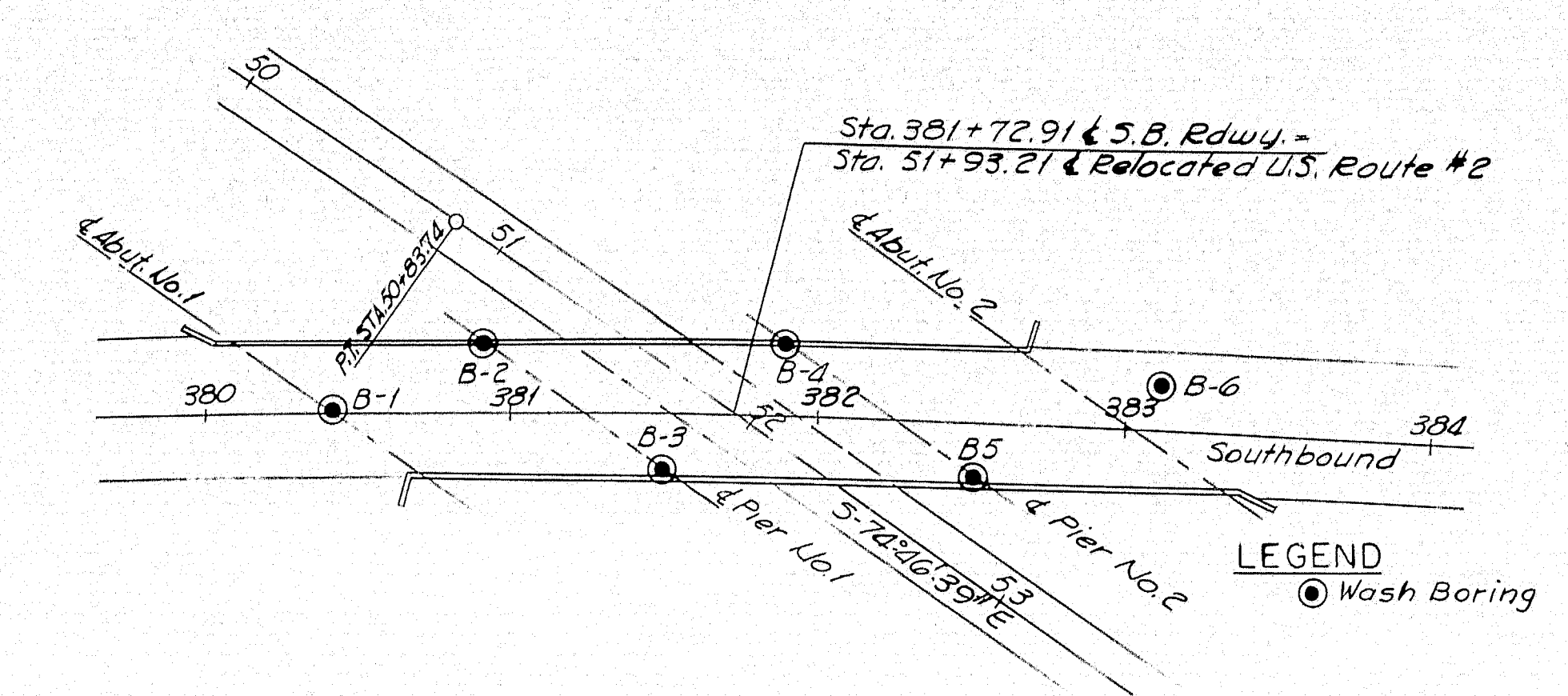
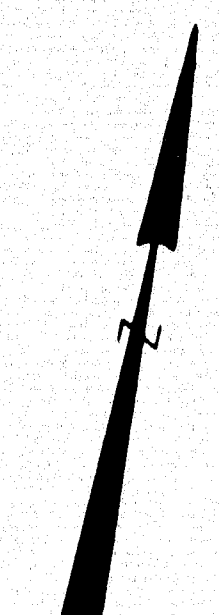
All concrete shall be Class "A" except slope paving which shall be class "B".



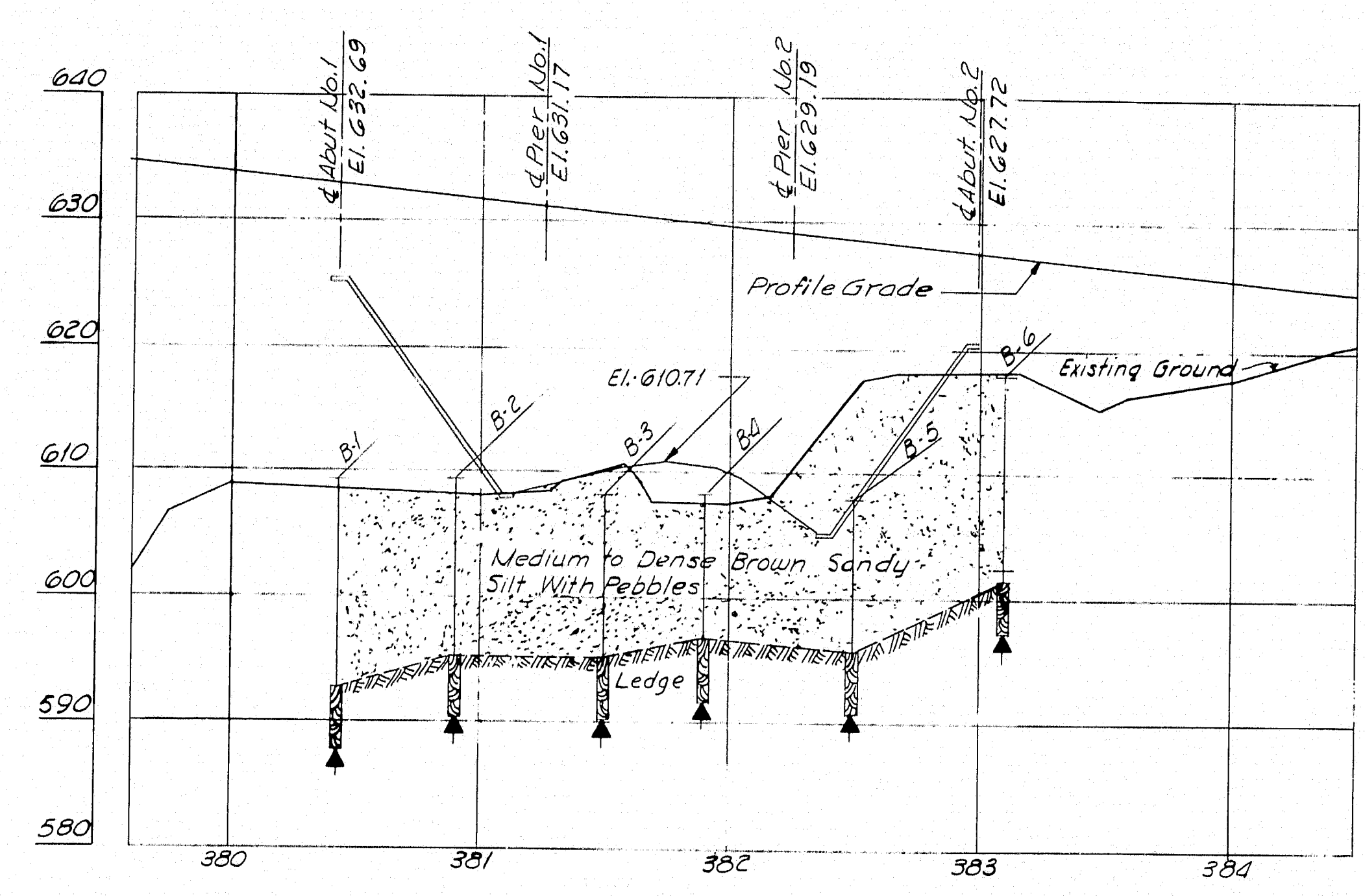
DESIGN - TRACE - CHECK - P.R.N.	DETAIL DAY	BRIDGE NO. SURVEY - PLOT -
STATE HIGHWAY COMMISSION BRIDGE DIVISION		
INTERSTATE 95 OVER U.S. ROUTE 2 IN THE TOWN OF SMYRNA ARROSTOOK COUNTY		
GENERAL PLAN		
SHEET 2 OF 18 AUGUSTA, MAINE MARCH 1965		

M-2223 SMYRNA (14)



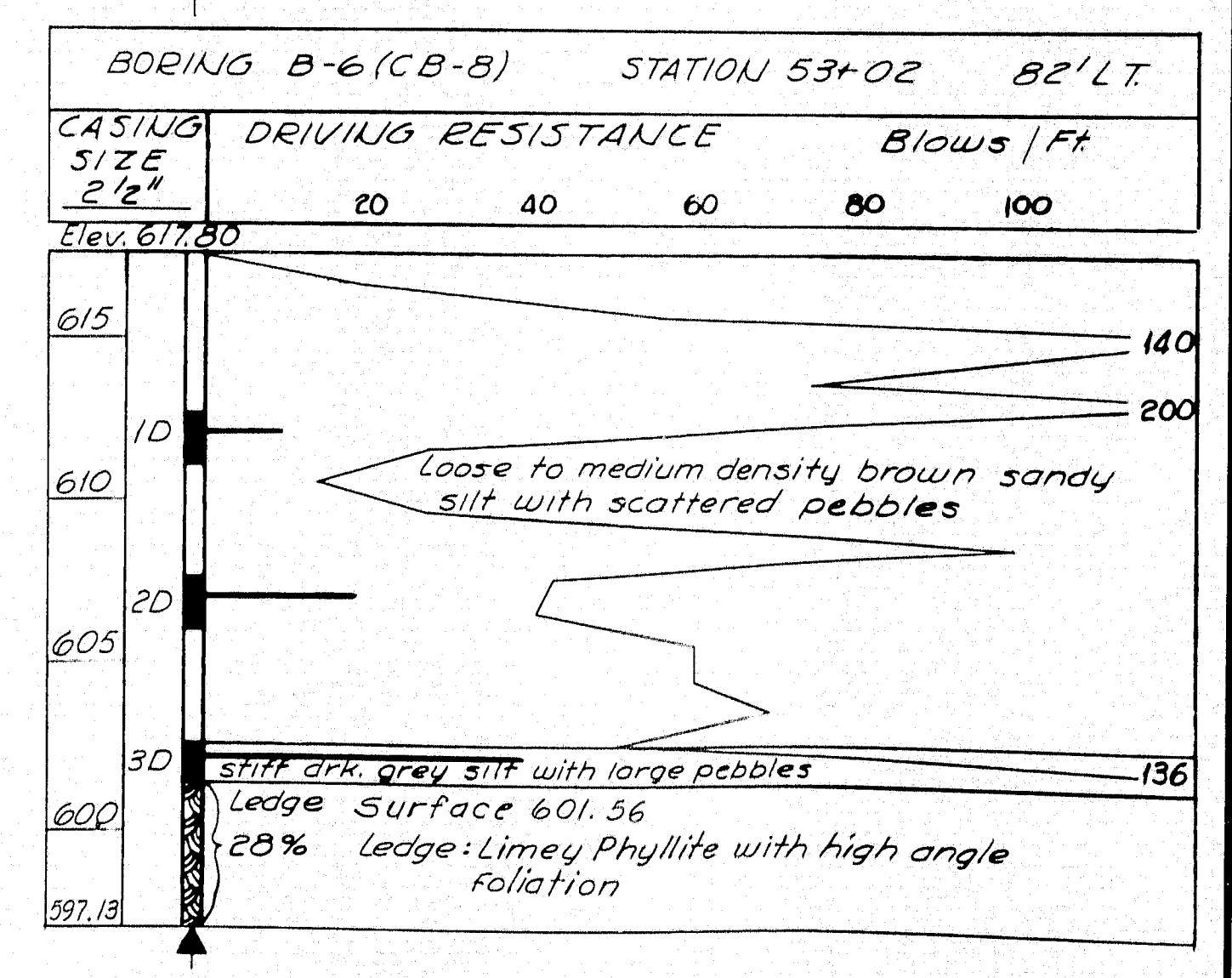
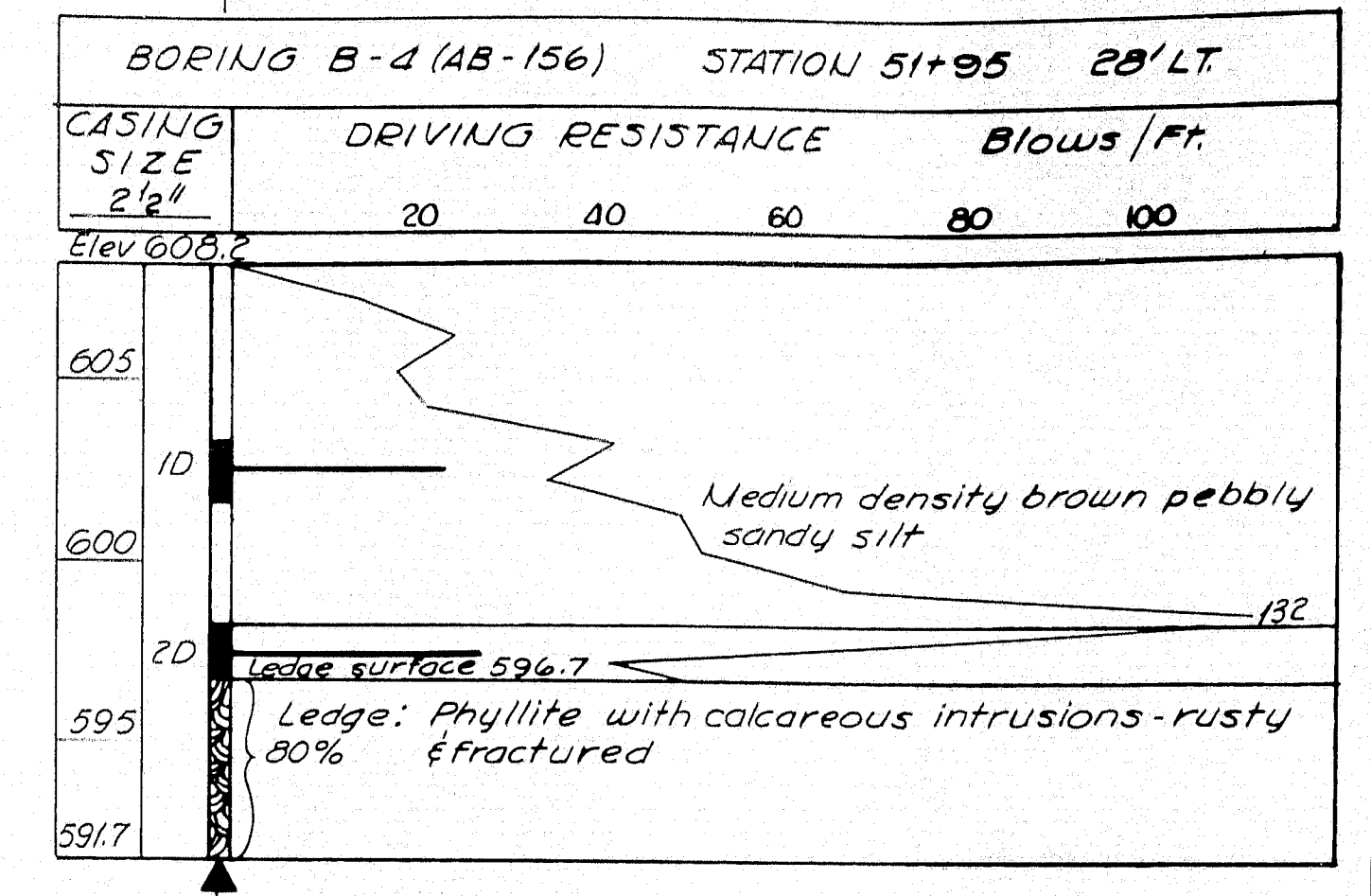
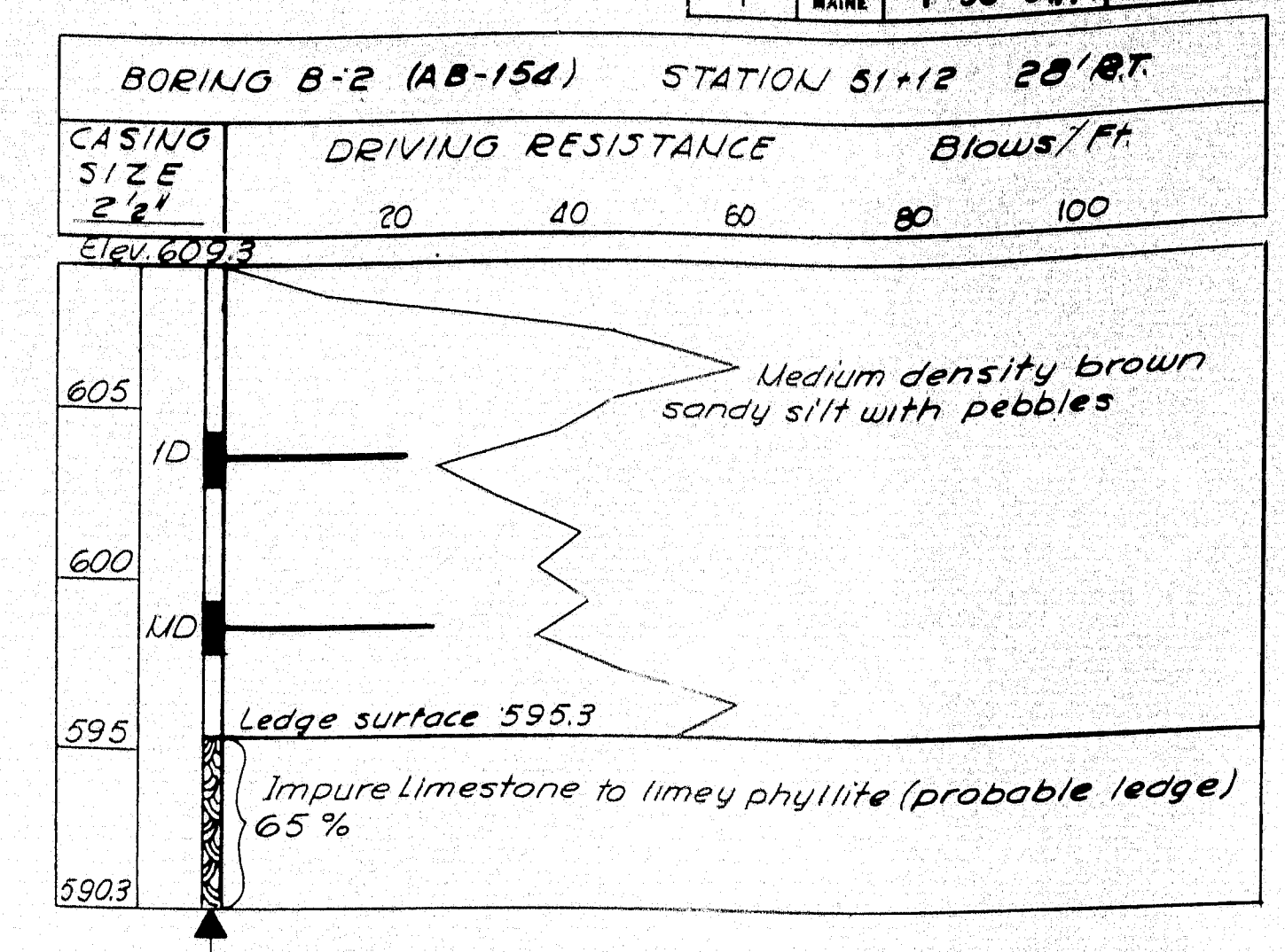
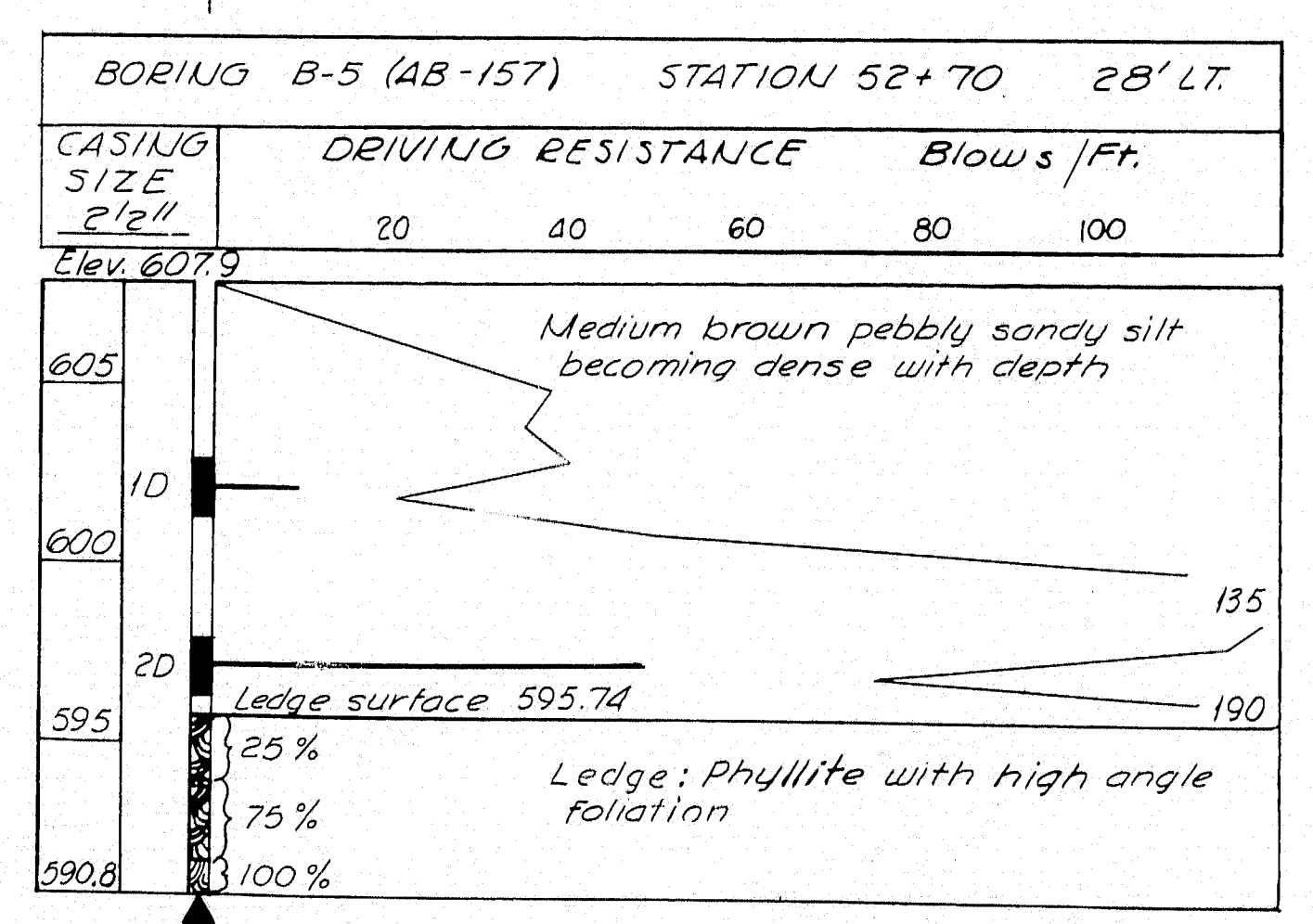
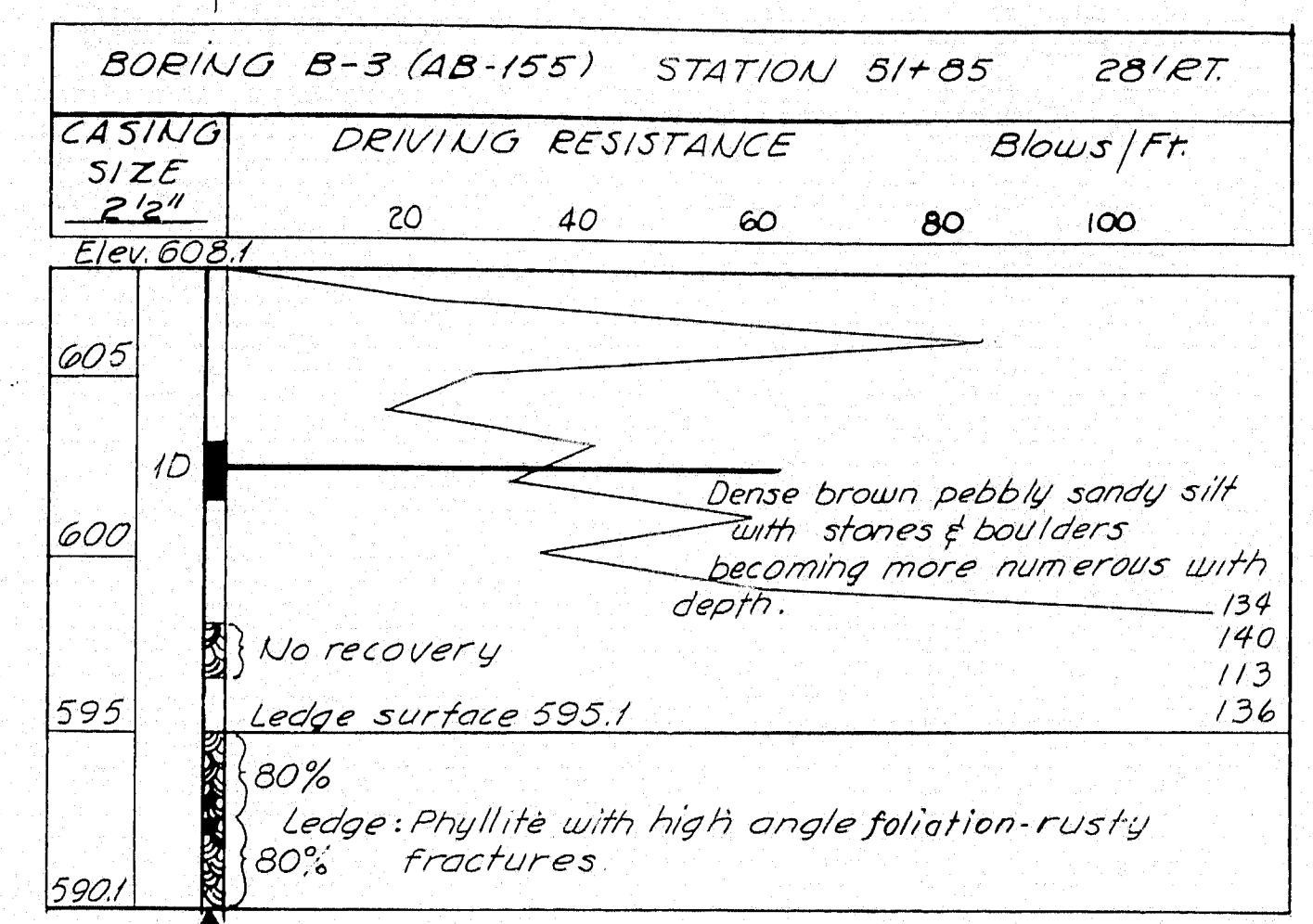
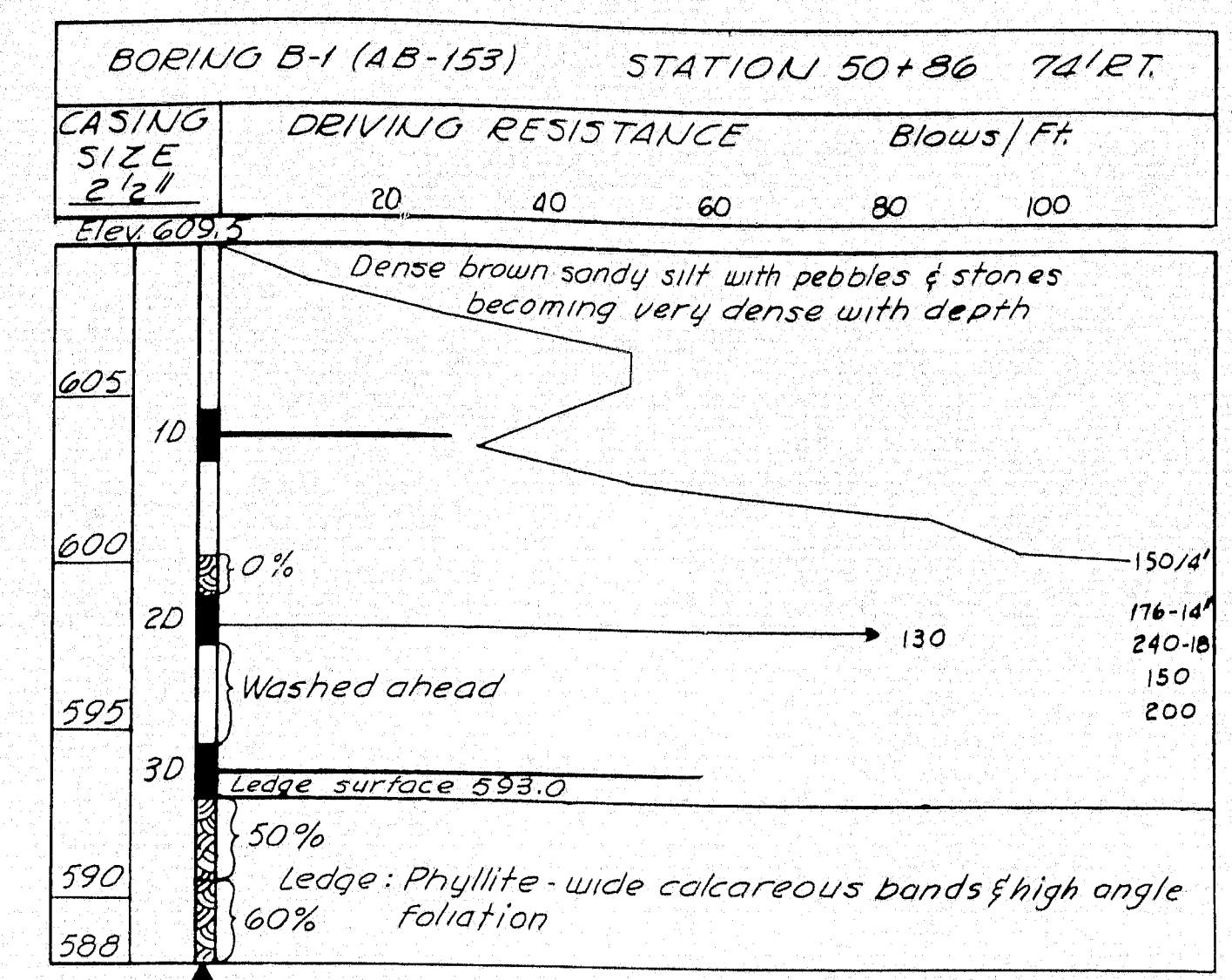


PLAN
1" = 50'



PROFILE SOUTHBOUND
1" = 10' Vert.
1" = 50' Horiz.

- NOTES:
- Number of blows required to drive extra heavy casing one foot with 400 ft. lbs of energy per blow.
 - Location of sample or sample attempt.
 - ID S.F.H. Sampler #1290's
 - MD Unsuccessful sample attempt and type of sampler
 - Number of blows required to drive spoon or tubing one foot with 350 ft. lbs of energy per blow.
 - Bottom of boring (may not be bottom of soil strata).
 - 71% Locations cored by diamond bit and per cent recovery of rock



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CONSULTING ENGINEERS
NEW YORK BOSTON KANSAS CITY

DESIGN -
TRACE -
CHECK - V.A.V.

DETAIL R.K.

BRIDGE NO.
SURVEY -
PLOT -

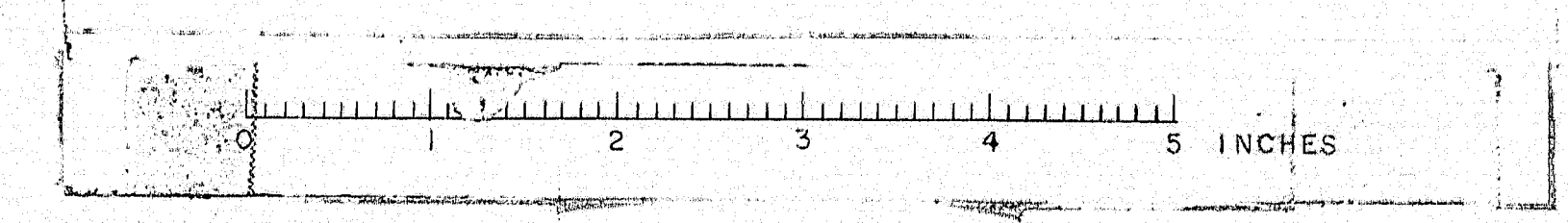
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

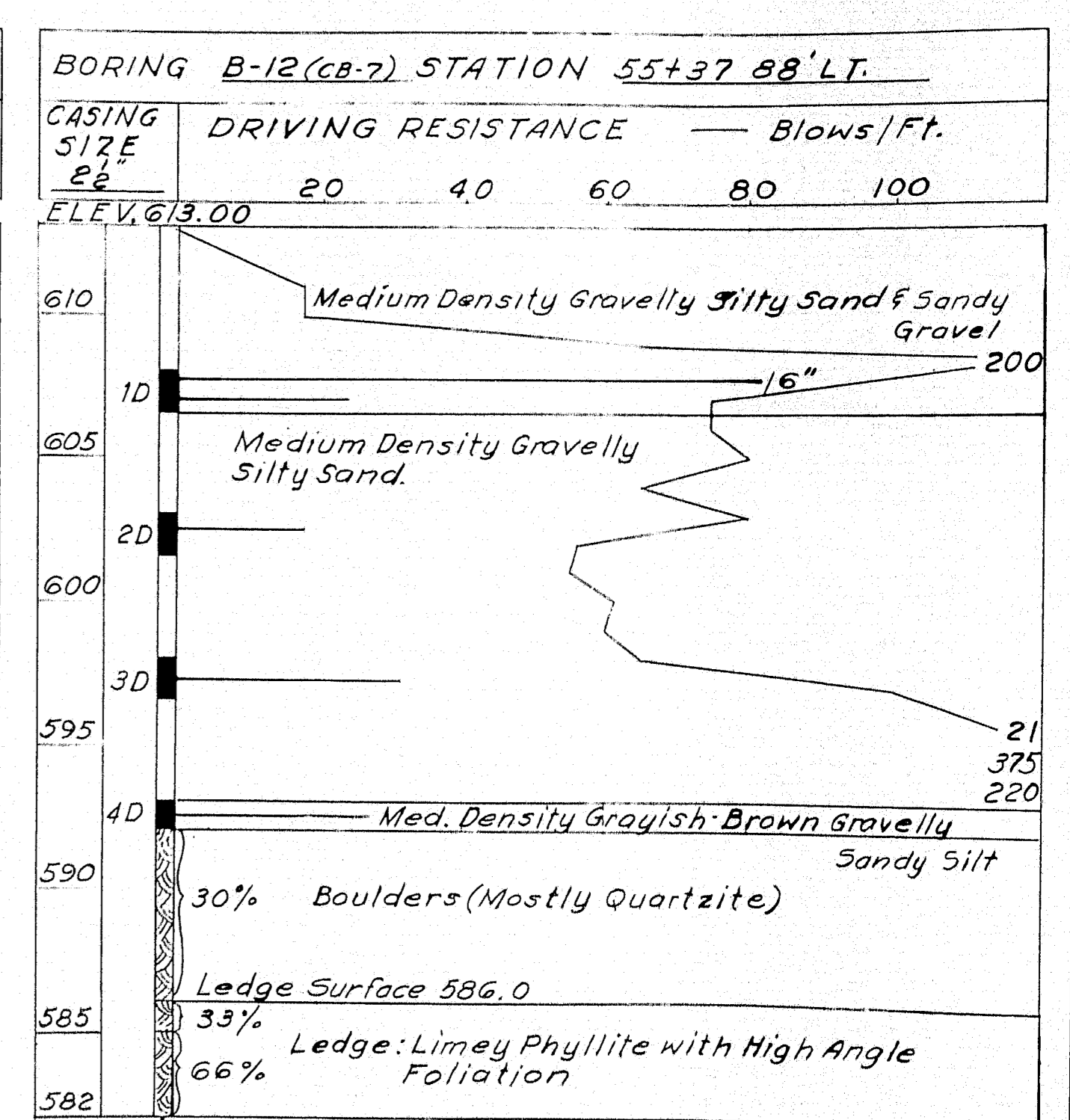
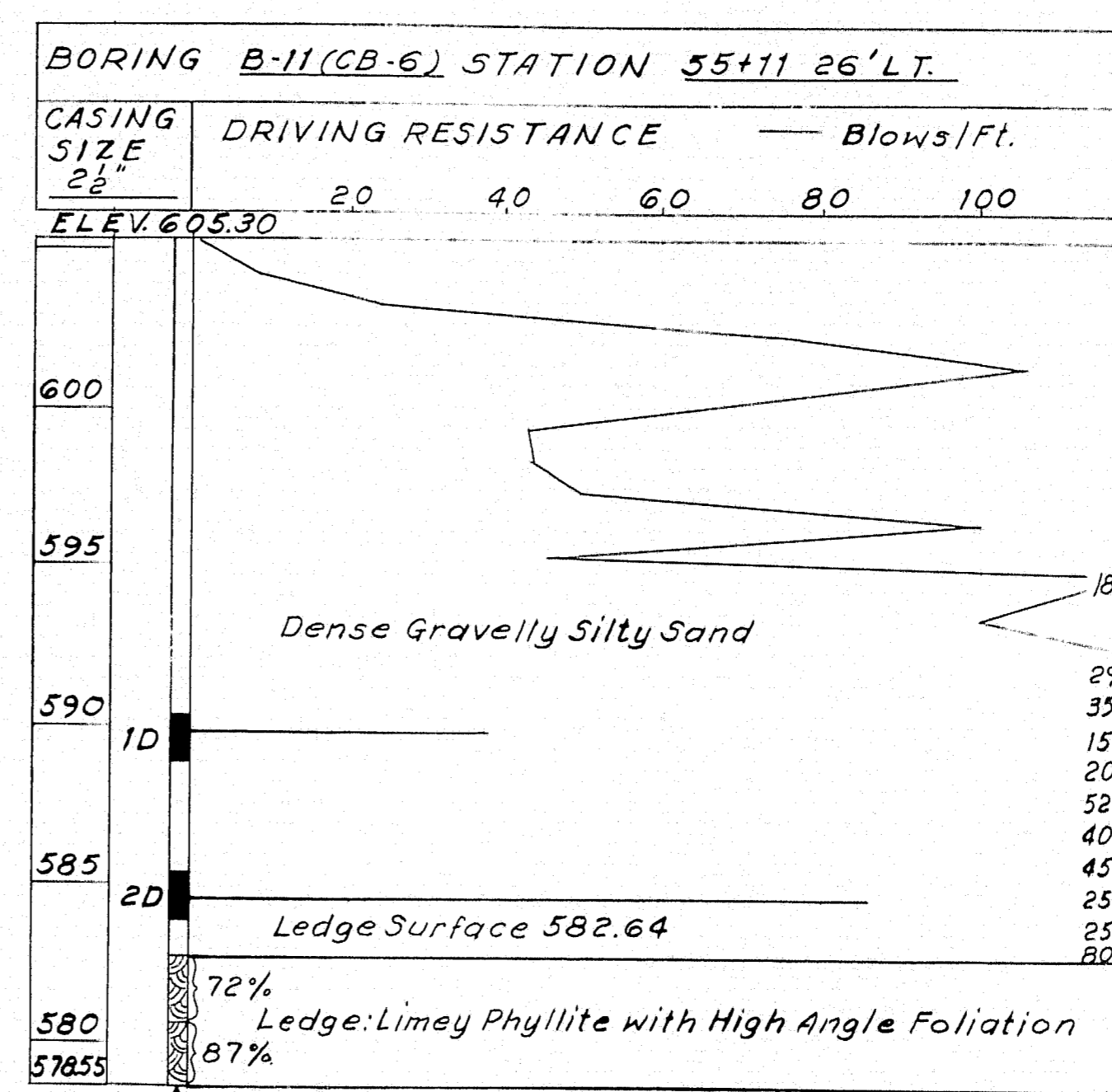
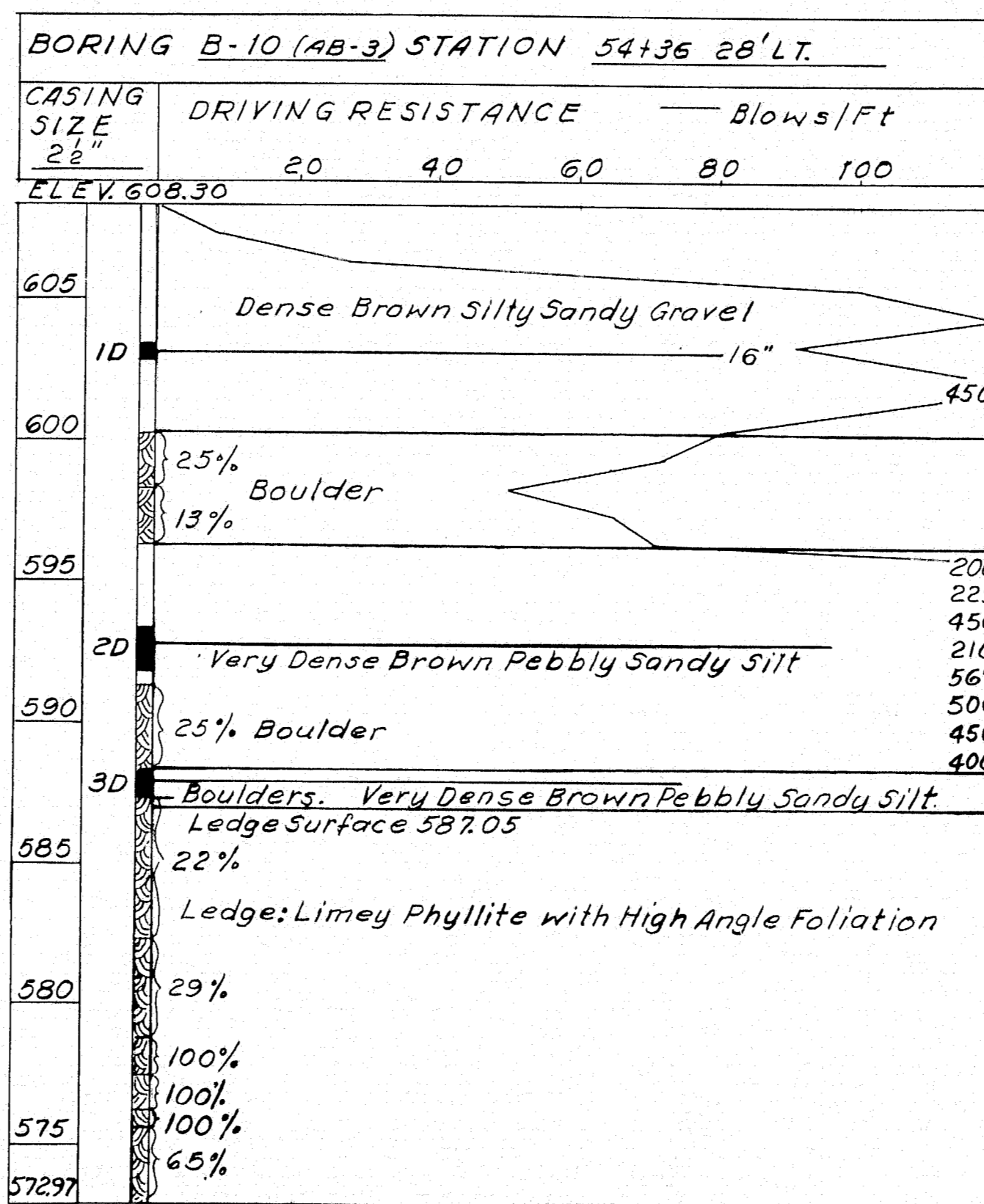
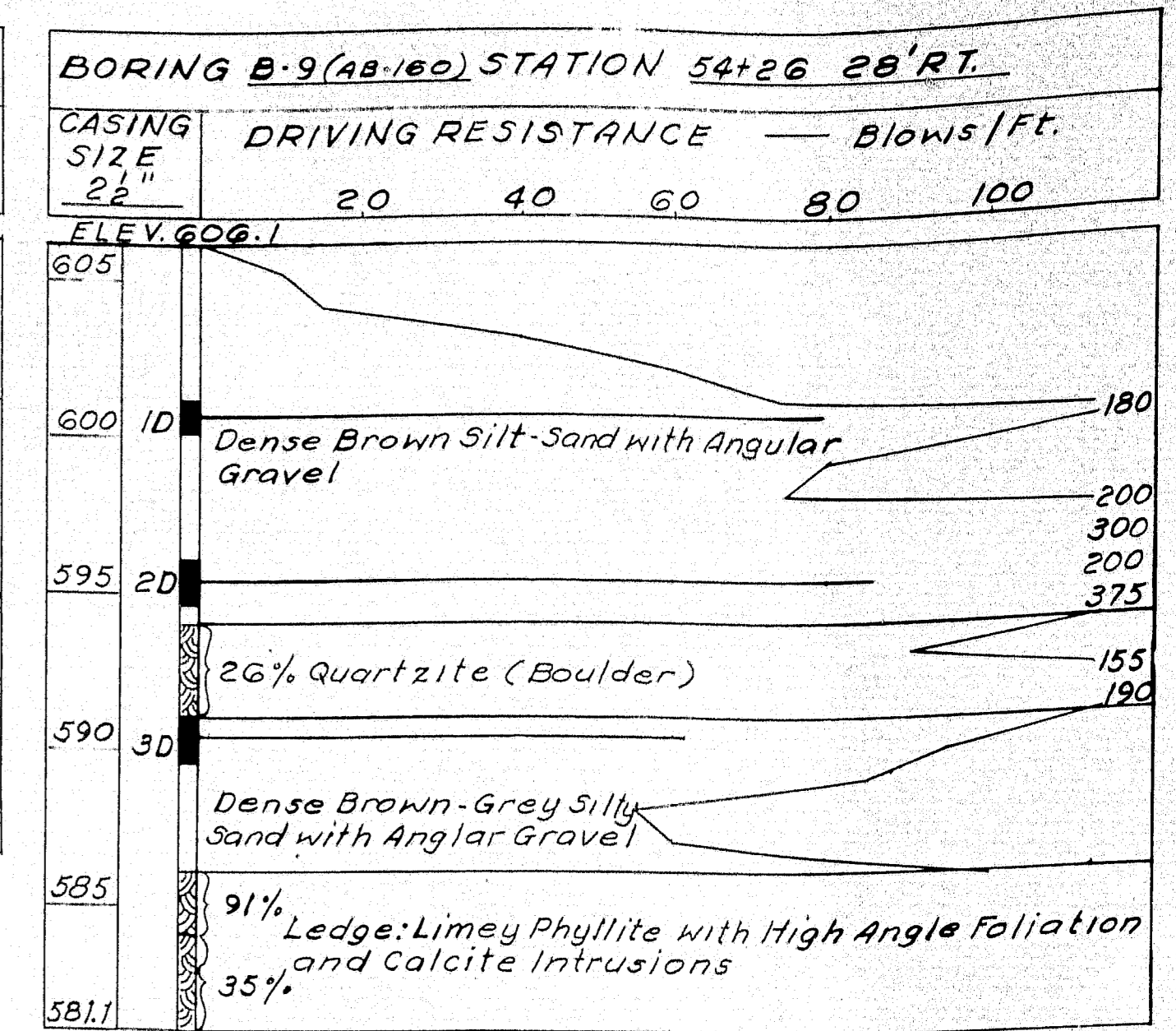
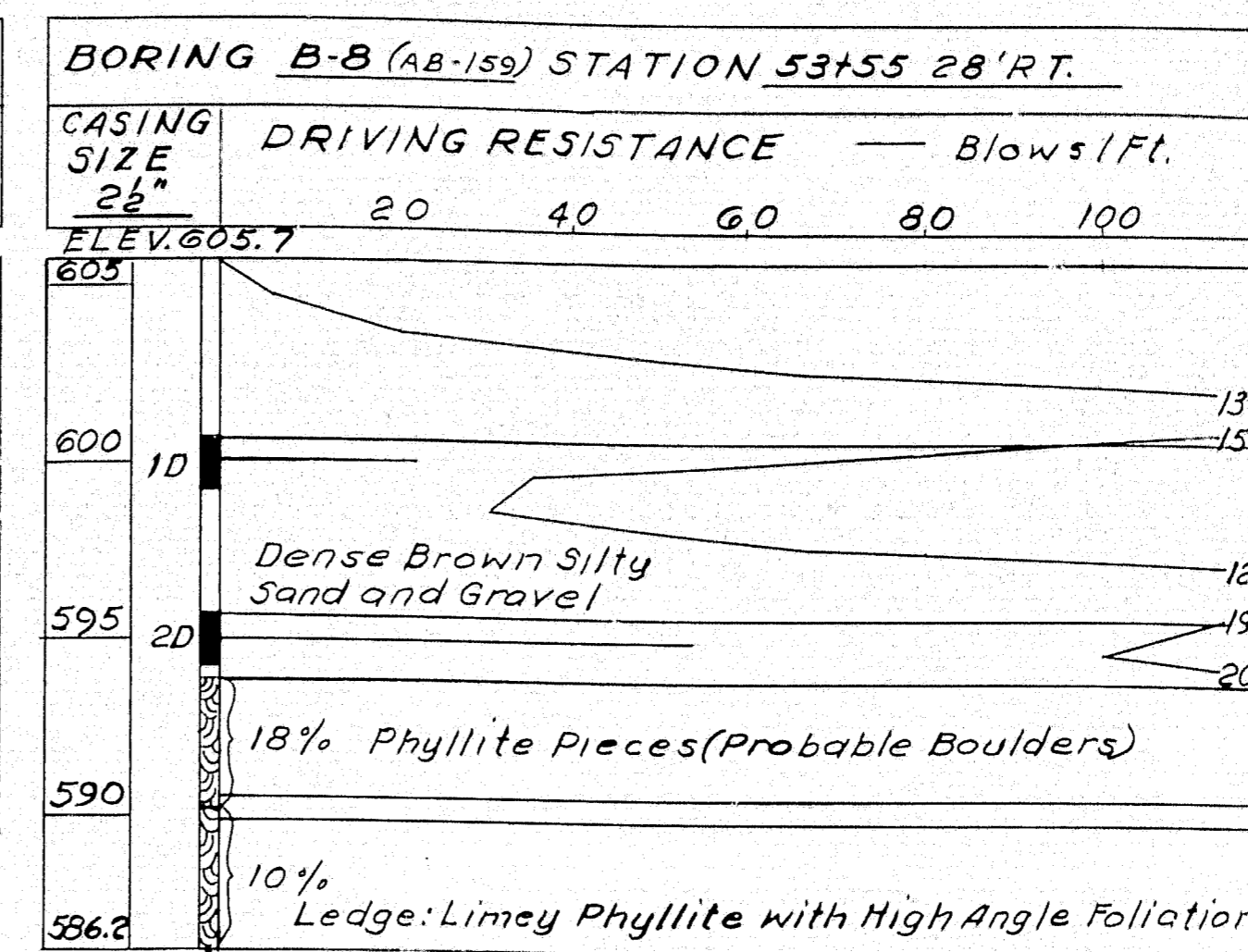
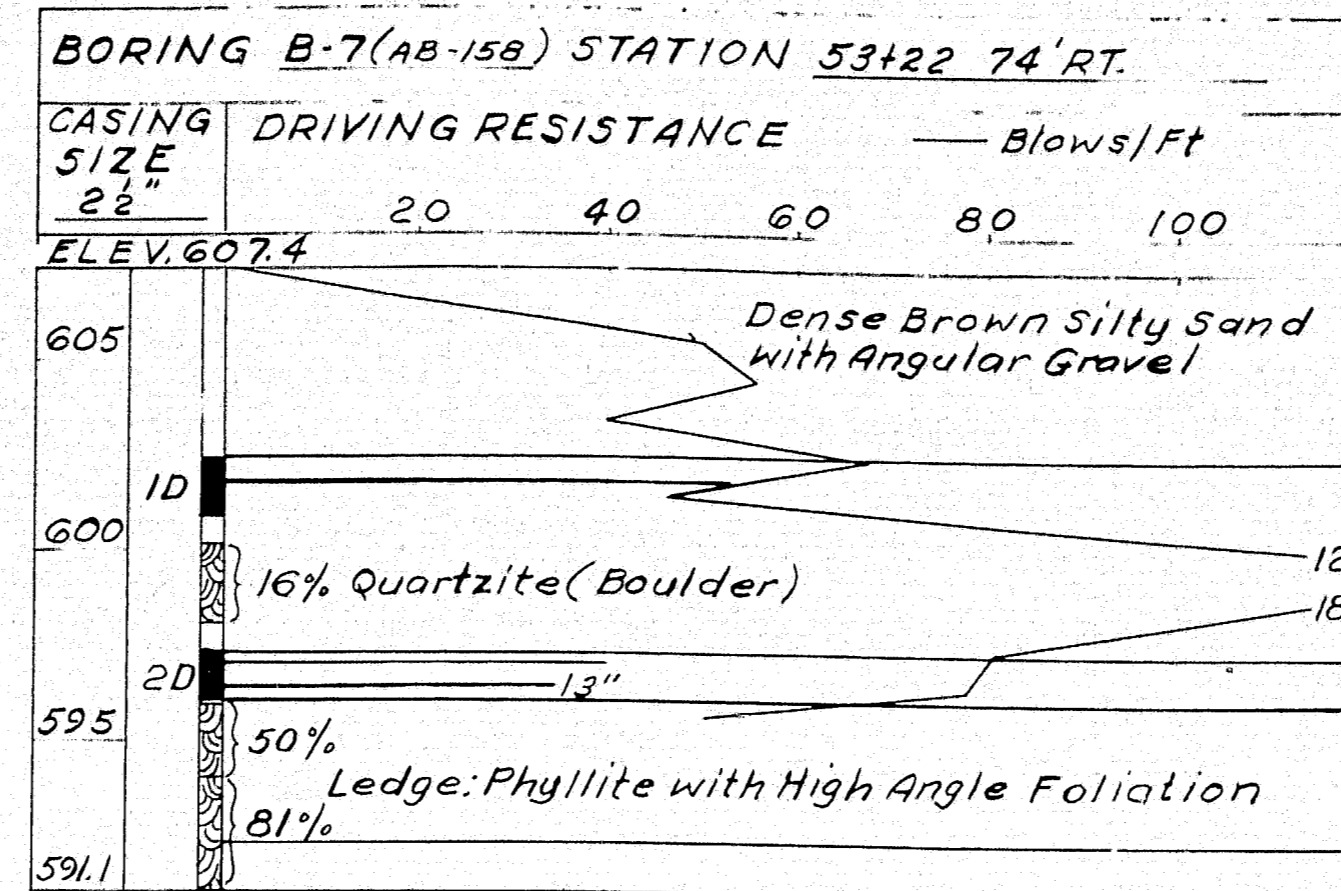
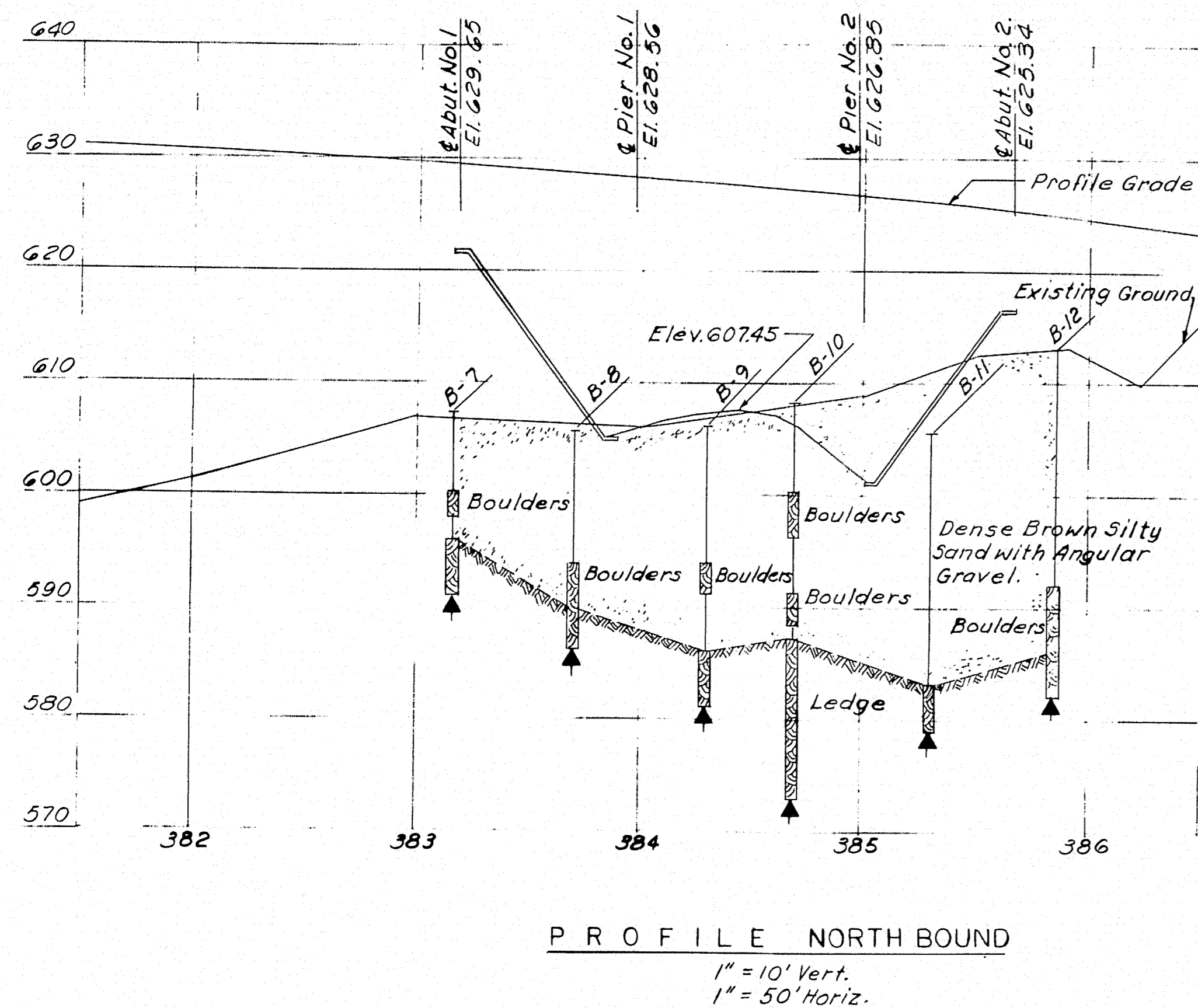
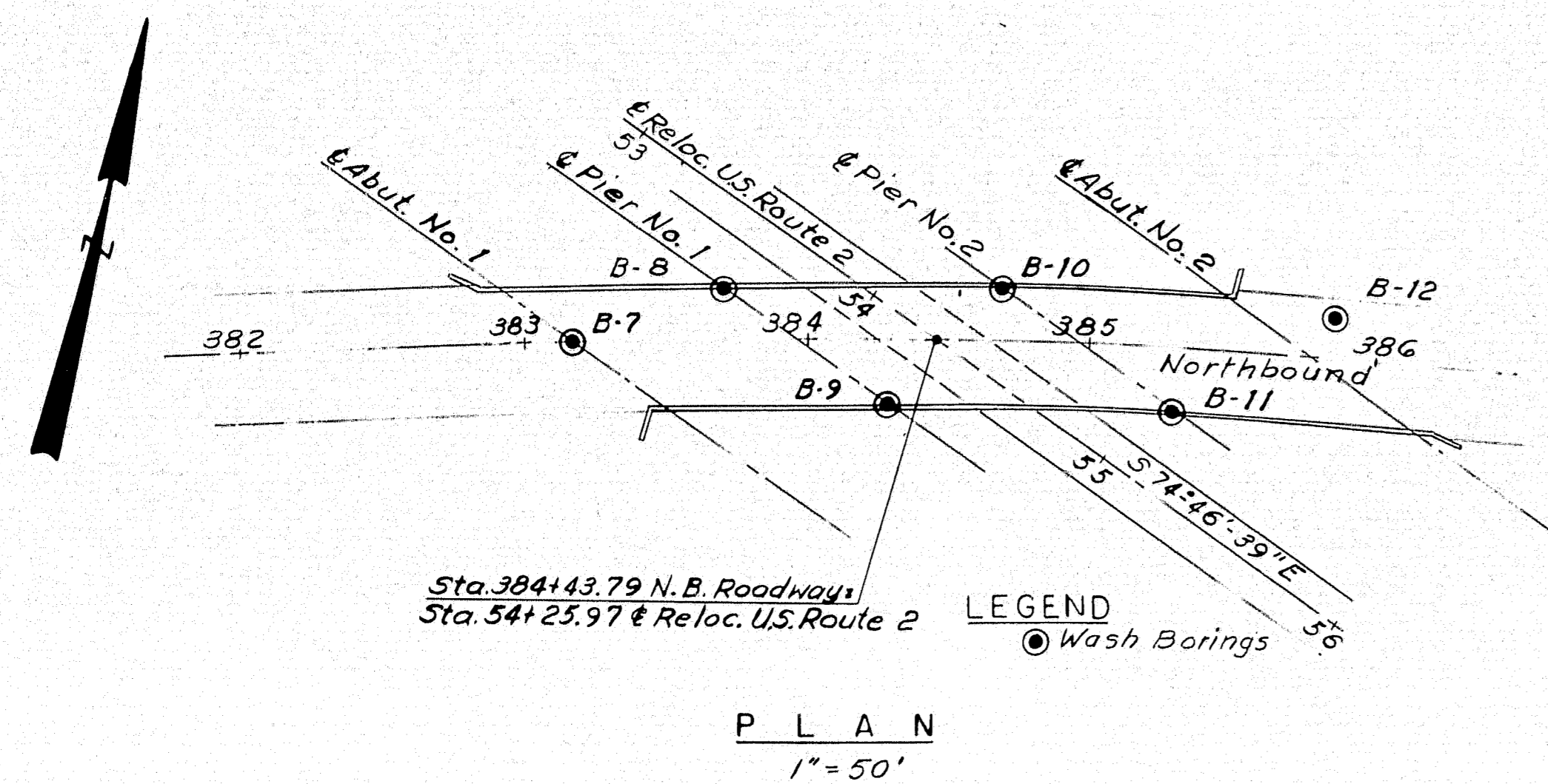
INTERSTATE 95 S.B.
OVER
U. S. ROUTE 2
IN THE TOWN OF
SMYRNA
AROOSTOOK COUNTY

FOUNDATION SURVEY

SHEET 3 OF 18 AUGUSTA, MAINE MARCH 1965

M-2224 SMYRNA (14)

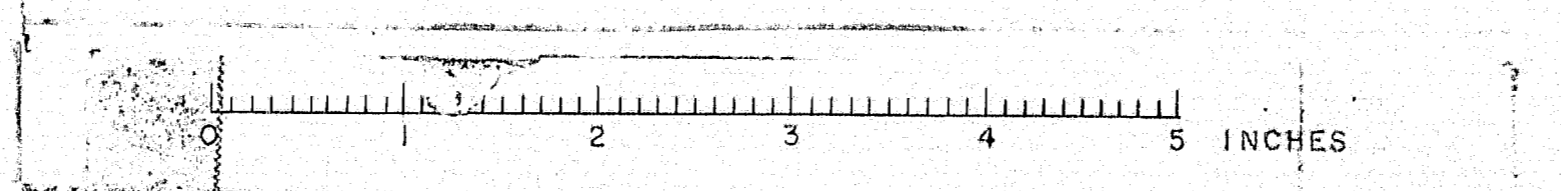


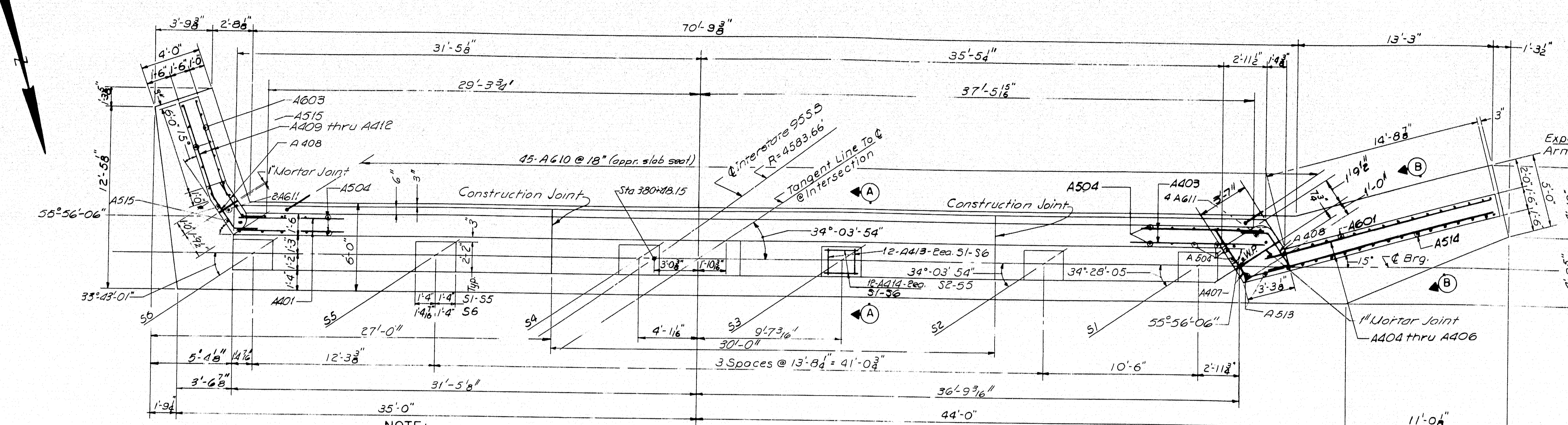


- NOTES:**
- Number of blows required to drive extra heavy casing one foot with 400 ft. lbs. of energy per blow.
 - Location of sample or sample attempt.
 - ID S. & H. Sampler #1290's
 - MD Unsuccessful sample attempt and type of sampler
 - Number of blows required to drive spoon or tubing one foot with 350 ft. lbs. of energy per blow.
 - Bottom of boring (may not be bottom of soil strata).
 - Locations cored by diamond bit and per cent recovery of rock.

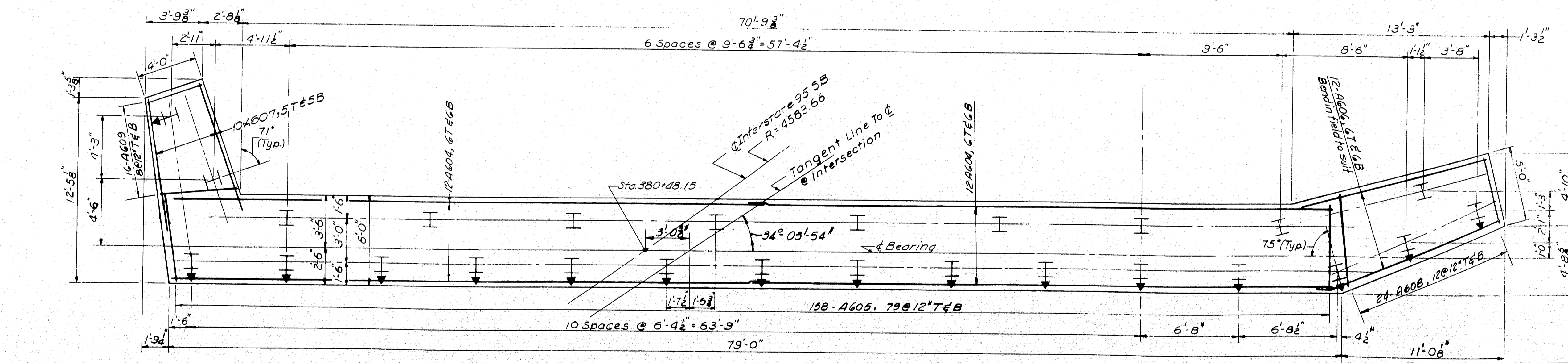
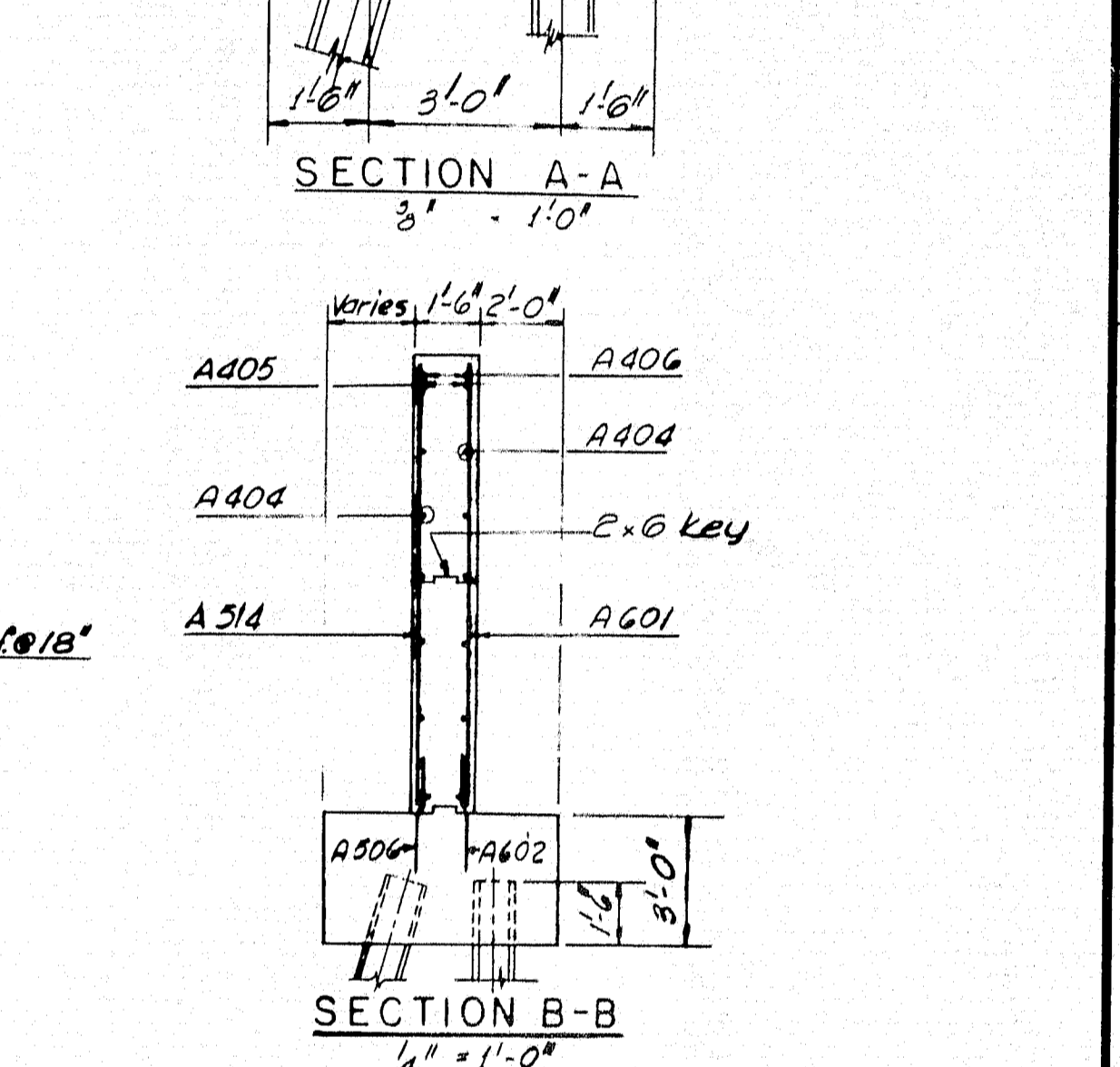
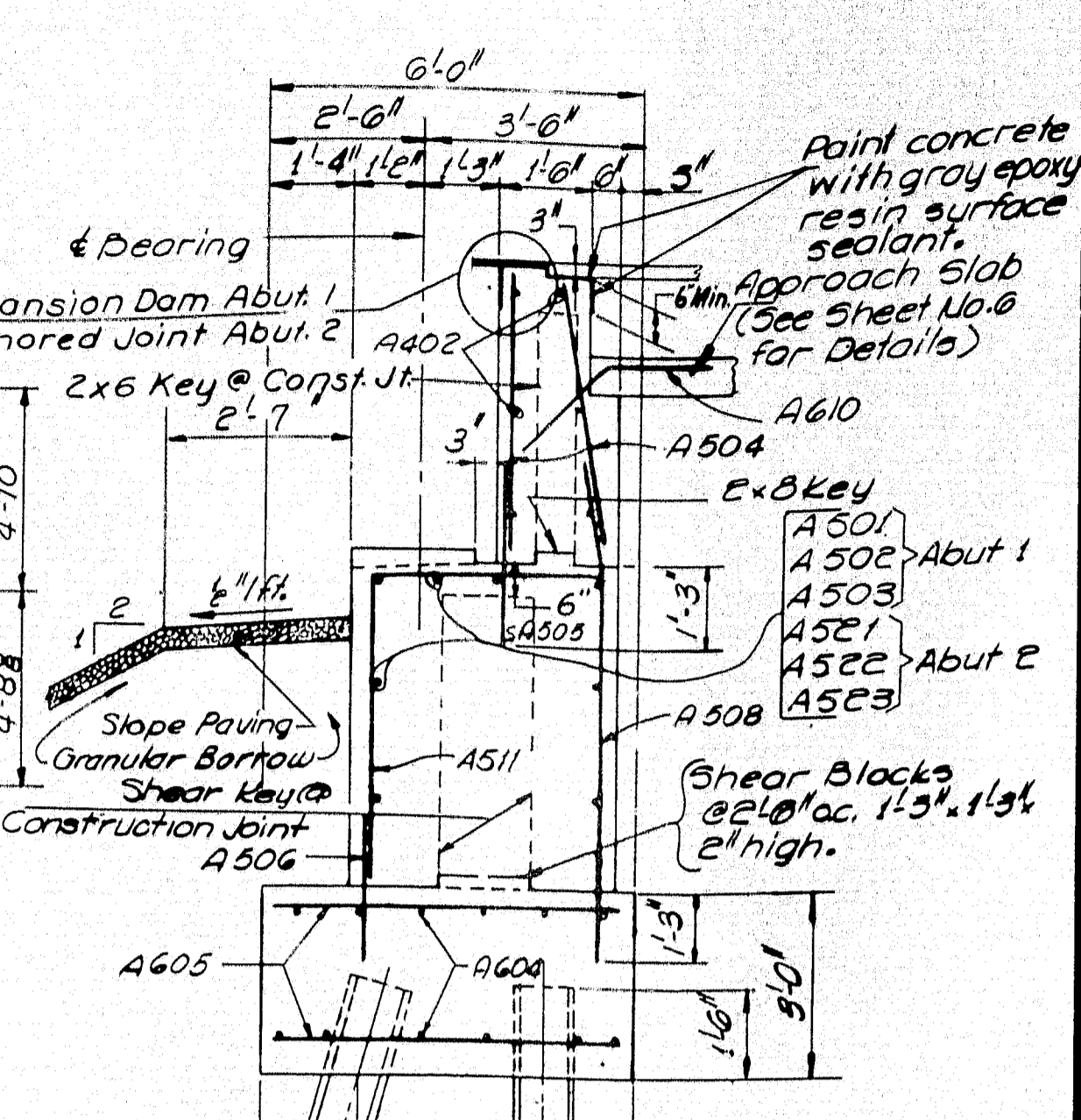
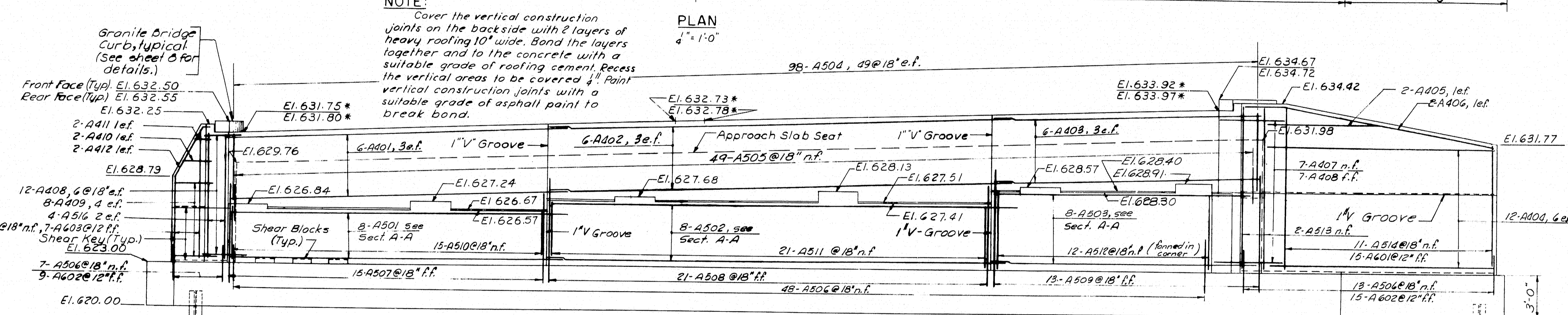
HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
NEW YORK BOSTON KANSAS CITY

DESIGN- TRACE- CHECK- V.A.V.	DETAIL G.E.C.	BRIDGE NO. SURVEY- PLOT-
STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 N.B. OVER U.S. ROUTE 2 IN THE TOWN OF SMYRNA ARROOSTOOK COUNTY FOUNDATION SURVEY SHEET 4 OF 18 AUGUSTA, MAINE MARCH 1965 SMYRNA (14)		





NOTE:
Cover the vertical construction joints on the backside with 2 layers of heavy roofing 10' wide. Bond the layers together and to the concrete with a suitable grade of roofing cement. Recess the vertical areas to be covered 1/4" Paint vertical construction joints with a suitable grade of asphalt paint to break bond.



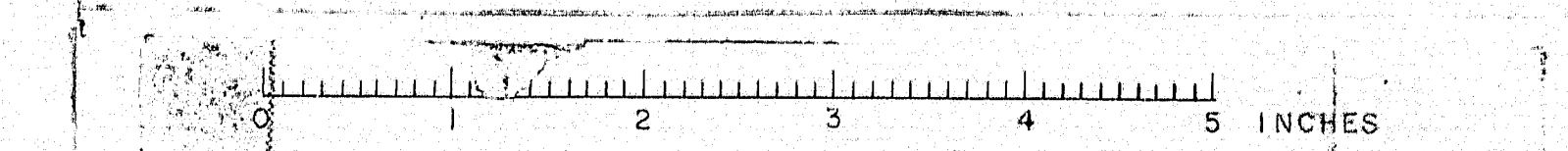
GENERAL NOTES:
1. For Approach Slab Details see Sheet No. 14.
2. Paint bridge seat, face of backwall and 1'-0" below top of slope paving on face and ends of breast wall with Gray Epoxy Resin Surface Sealant.
3. Dress bearing areas 1" larger all around than masonry plates to exact elevations.
4. Reinforcing steel to have 3" minimum cover unless otherwise shown.
5. For Pile Notes, see Sheet No. 6.
6. For Pile Point Detail, see Sheet No. 6.
7. e.f. = each face, n.f. = near face, f.f. = far face.
8. Place reinforcing to clear anchor bolts.

STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 SB
OVER
U.S. ROUTE 2
IN THE TOWN OF
SMYRNA
AROOSTOOK COUNTY
ABUTMENT NO. 1

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
NEW YORK BOSTON KANSAS CITY

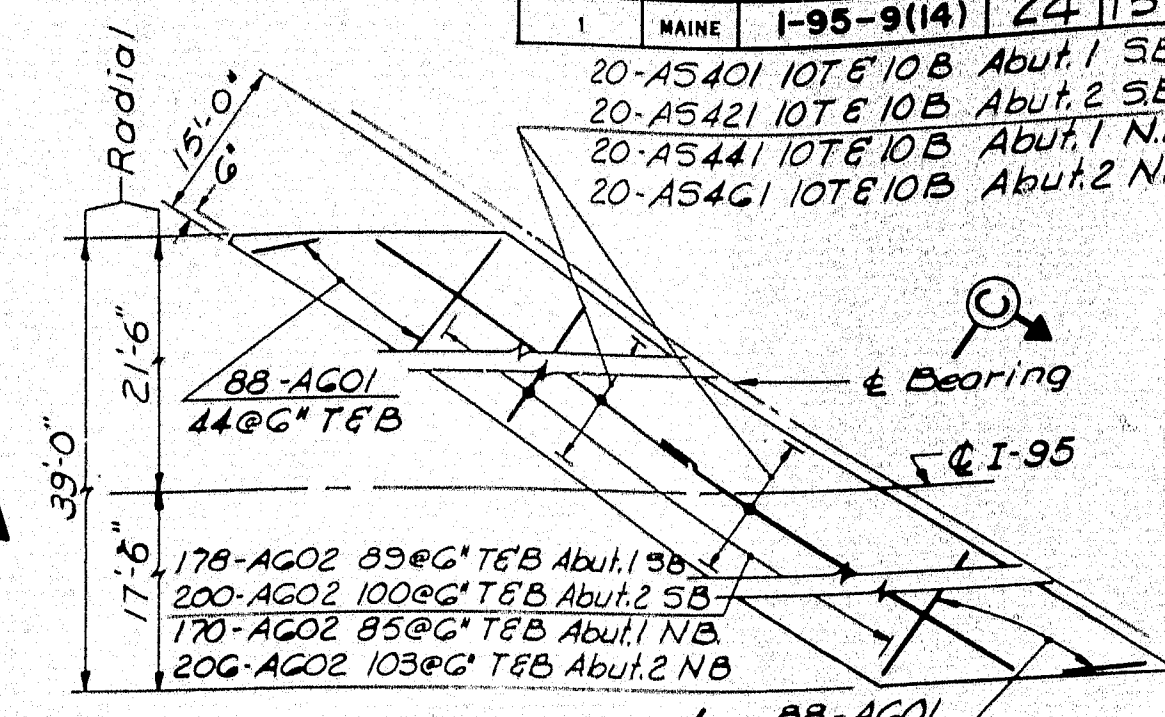
SHEET 5 OF 18 AUGUSTA, MAINE MARCH 1965

M-2226 SMYRNA(14)

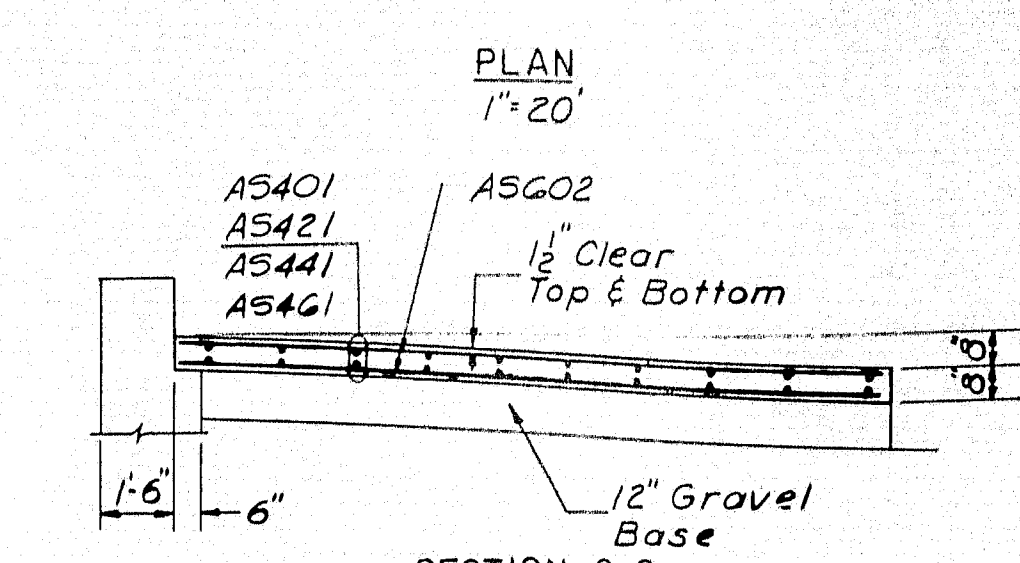


B. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-9(14)	24	158

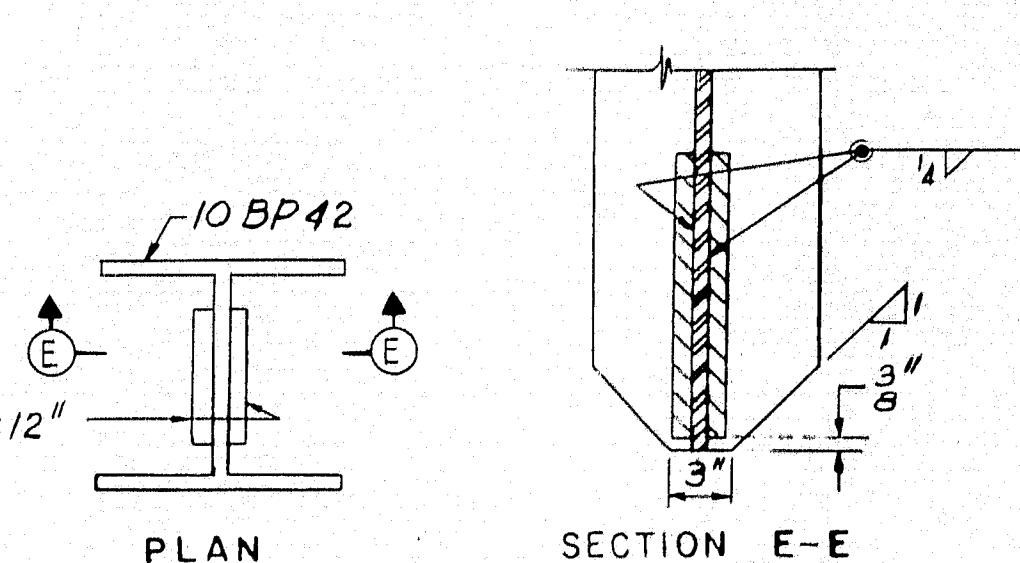
20-A5401 10T 5' 10B Abut. 1 SB.
20-A5421 10T 5' 10B Abut. 2 SB.
20-A5441 10T 5' 10B Abut. 1 NB.
20-A5461 10T 5' 10B Abut. 2 NB.



NOTE:
Approach Slab Concrete will be paid for under Item 701-33. Portland Cement Concrete Abutments and Retaining Walls.



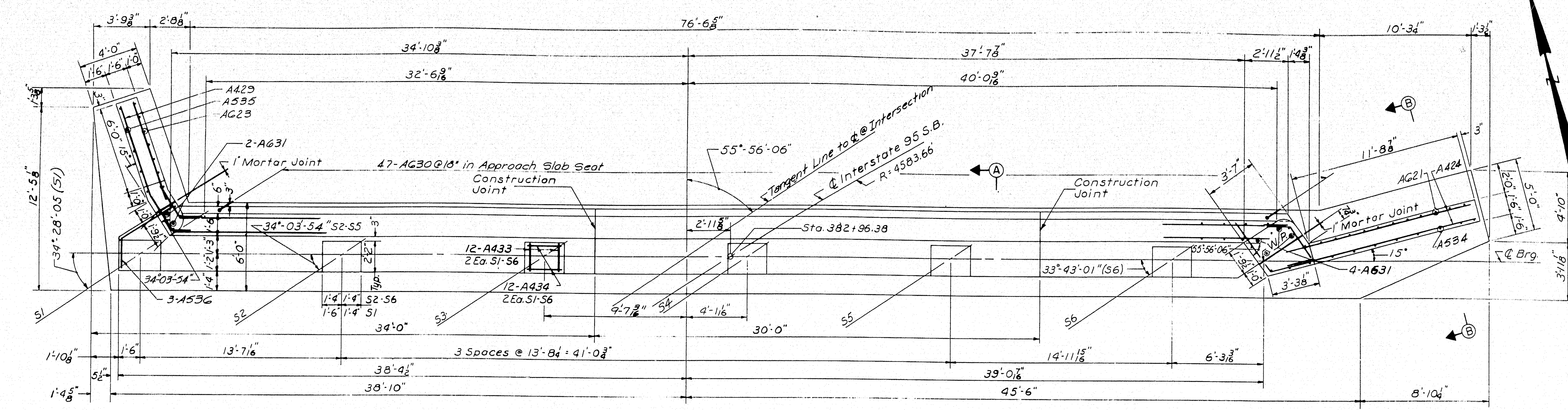
APPROACH SLAB DETAILS



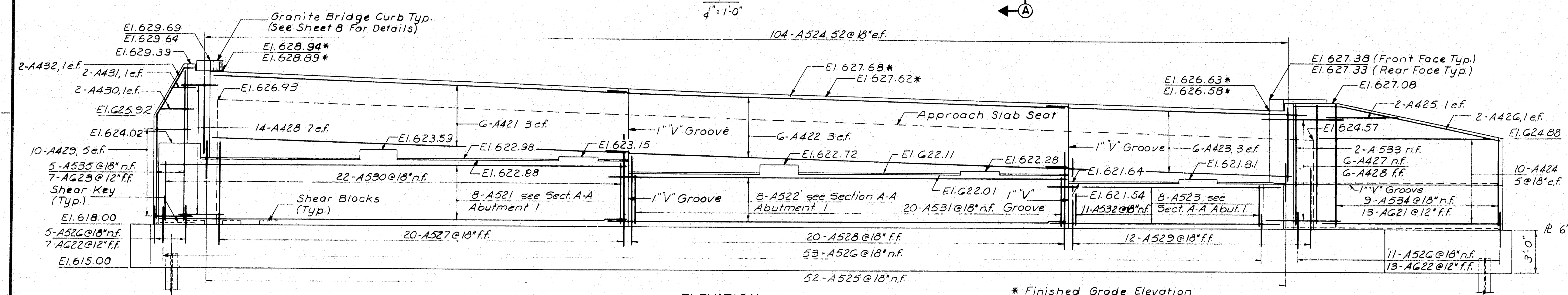
PILE POINT DETAIL

NOTES:
1. For General Notes see Sheet No. 5.
2. For Section A-A and B-B see Sheet No. 5.
3. Cover the vertical construction joints on the backside with 2 layers of heavy roofing 10' wide. Bond the layers together and to the concrete with a suitable grade of roofing cement. Recess the vertical areas to be covered 1'. Paint vertical construction joints with a suitable grade of asphalt paint to break bond.
PILE NOTES:
1. Indicates Vertical Piles.
2. Indicates Battered Piles, with 3:12 batter in the direction of arrow.
3. All piles 10BP42 with 37 ton capacity.
4. Estimated pile length, 30' ft. Abut. 1 & 21' ft. Abut. 2.
5. Piles to be driven to refusal or practical refusal to develop end bearing.

DESIGN - E.F.K. DETAIL - R.B.F.
TRACE - SURVEY - P.R.N.
BRIDGE NO. 100-100
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 SB
OVER
US ROUTE 2
IN THE TOWN OF
SMYRNA
AROOSTOOK COUNTY
ABUTMENT NO. 2
SHEET 6 OF 18 AUGUSTA, MAINE MARCH 1965

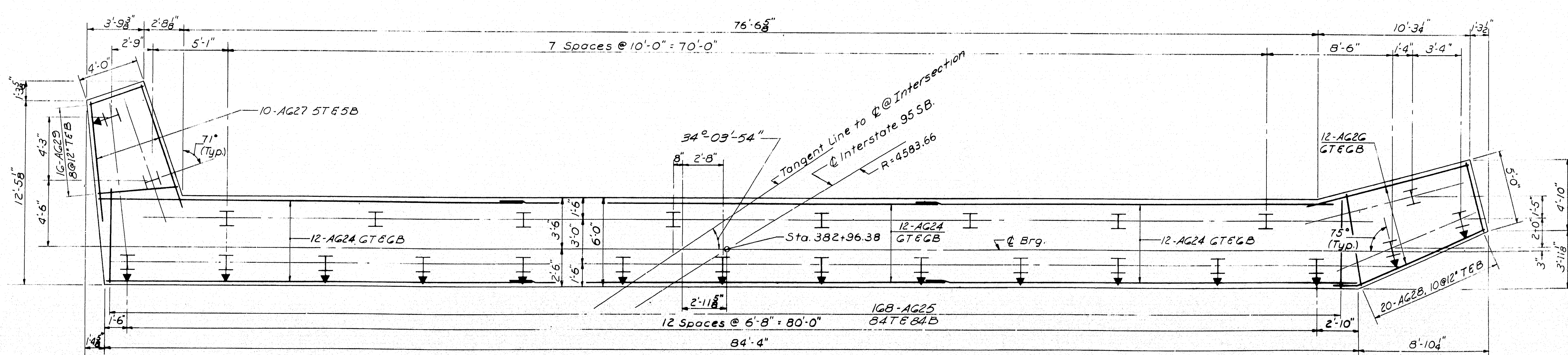


PLAN
4" = 1'-0"

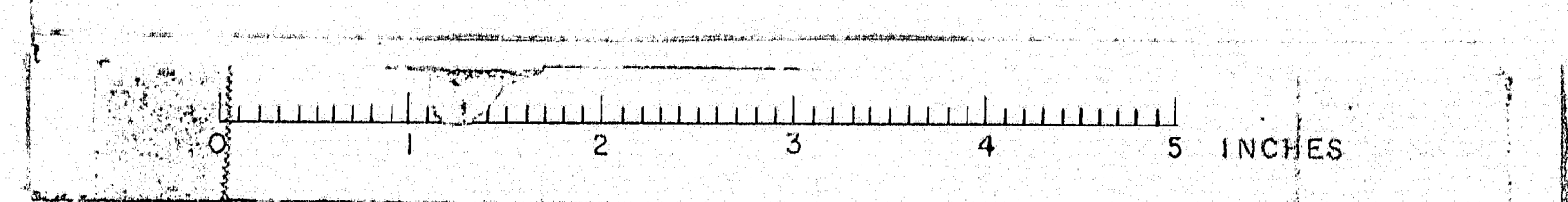


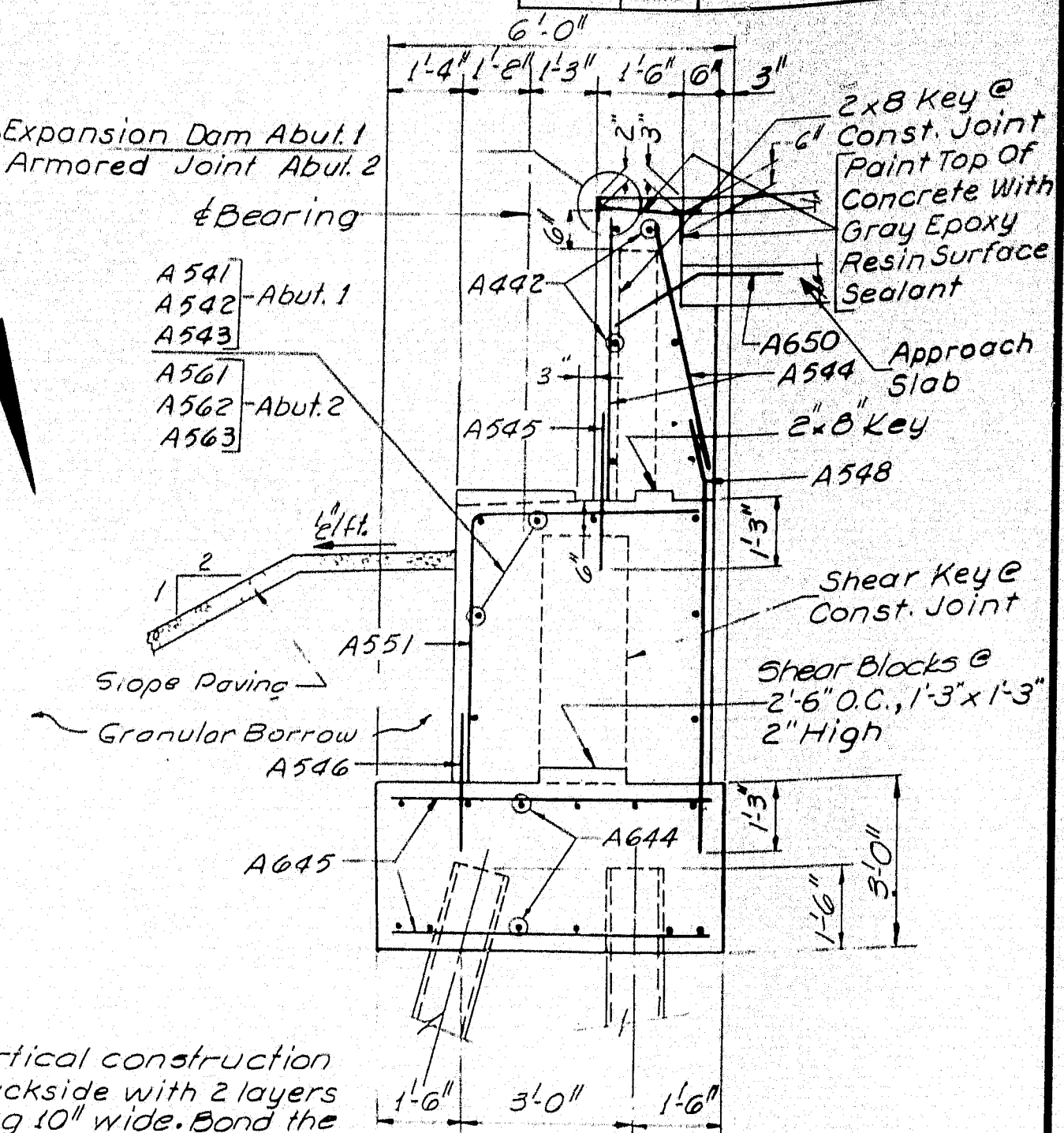
ELEVATION
4" = 1'-0"

* Finished Grade Elevation



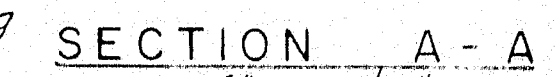
FOOTING & PILE PLAN
4" = 1'-0"



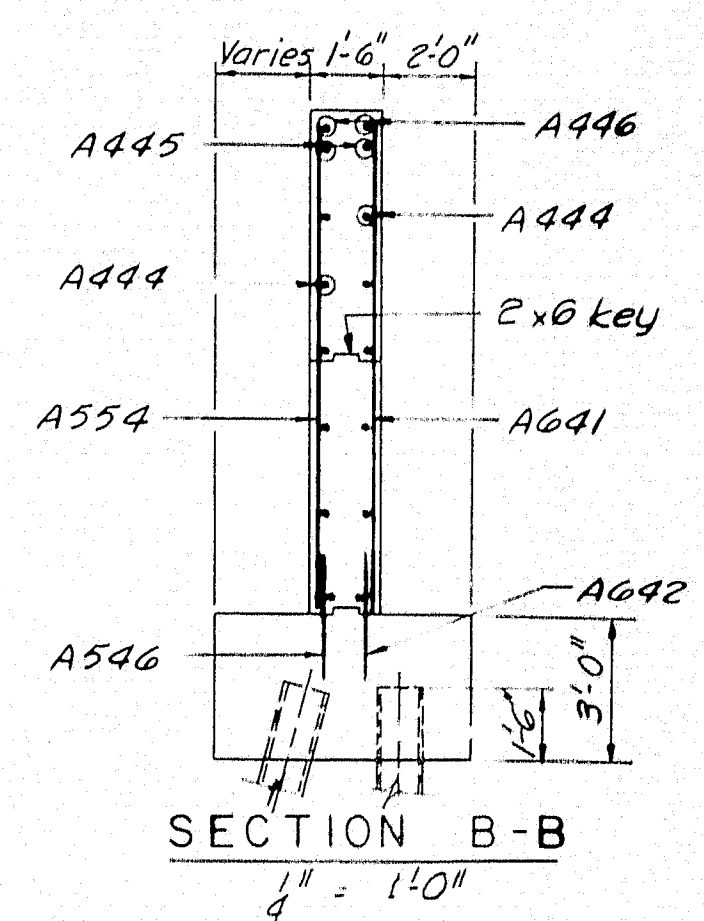


NOTE:

Cover the vertical construction joints on the backside with 2 layers of heavy roofing 10" wide. Bond the layers together and to the concrete with a suitable grade of roofing cement. Recess the vertical ST areas to be covered $\frac{1}{4}$ inch. Paint the vertical construction joints with a suitable grade of asphalt paint to break the bond.



* Finished Grade Elevation



NOTES:

1. For General Notes see sheet No. 5.
2. For Pile Notes see sheet No. 8

DESIGN— E.F.K. DETAIL— R.F.F. BRIDGE NO. _____
TRACE— _____ SURVEY— _____
CHECK— V.A.W. PLOT— _____

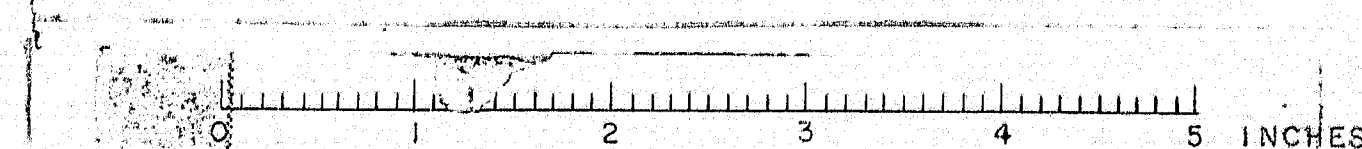
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95 NB
OVER
U.S. ROUTE 2
IN THE TOWN OF
SMYRNA
ARROSTOOK COUNTY

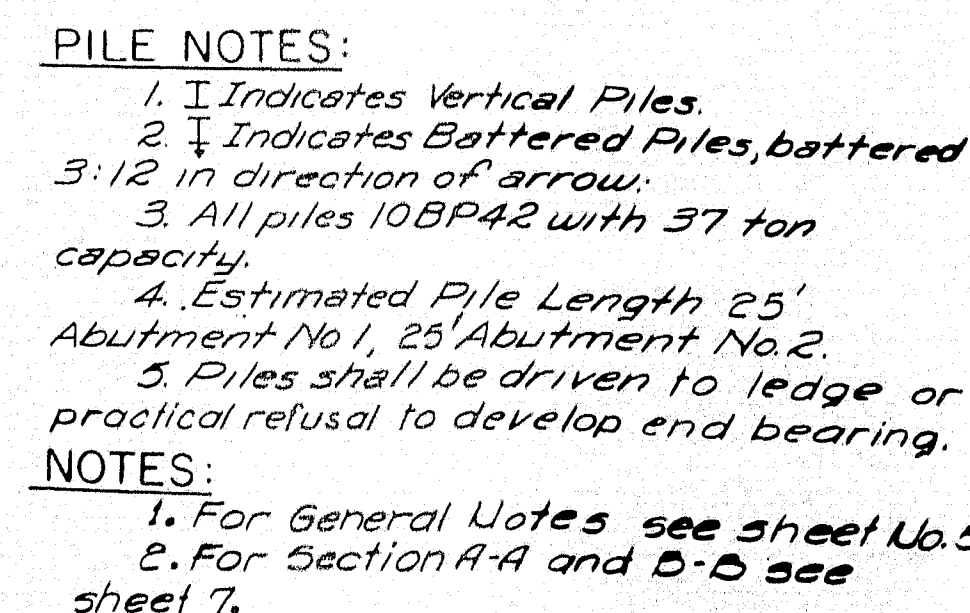
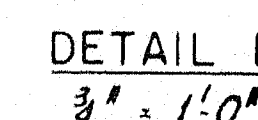
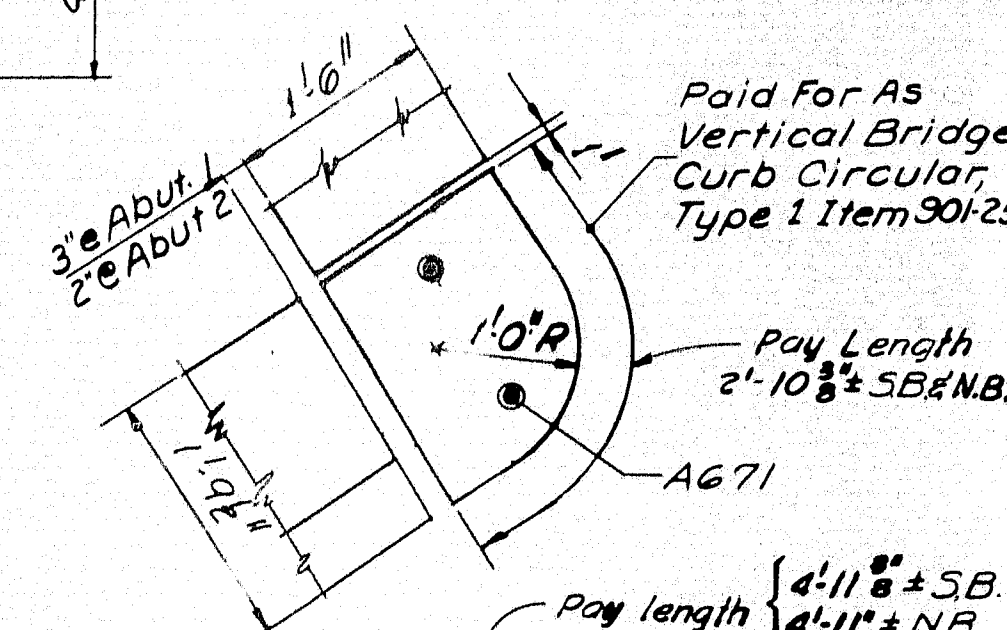
ABUTMENT NO. 1

SHEET 7 OF 18 AUGUSTA, MAINE MARCH 1965

M-2228 SMYRNA (14)

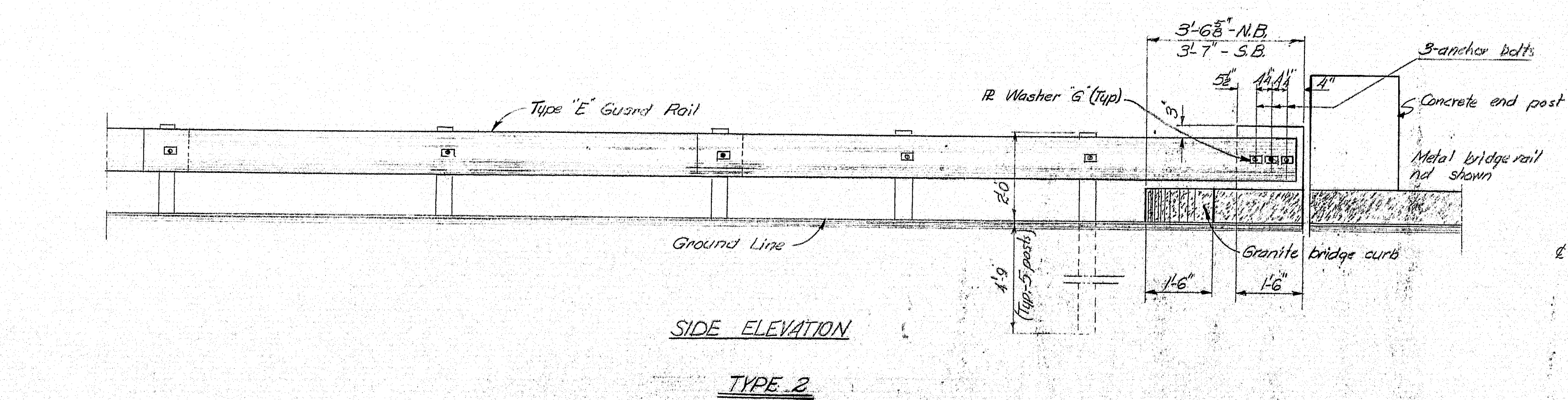
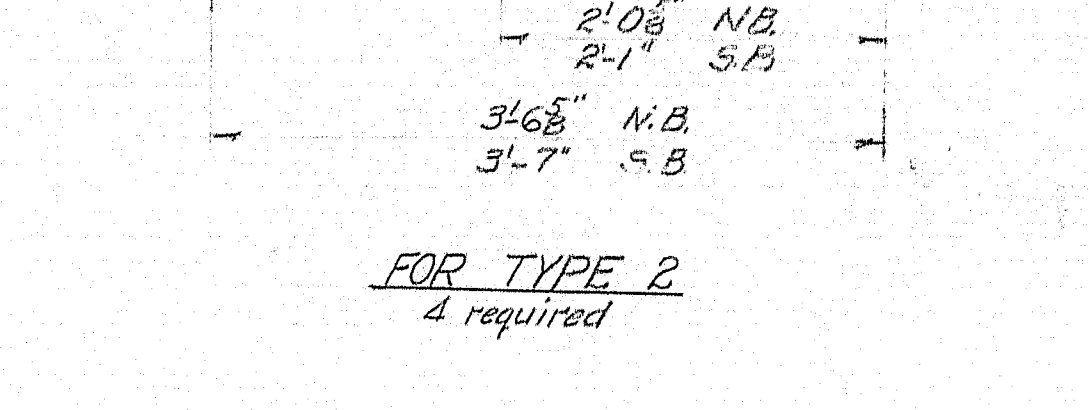
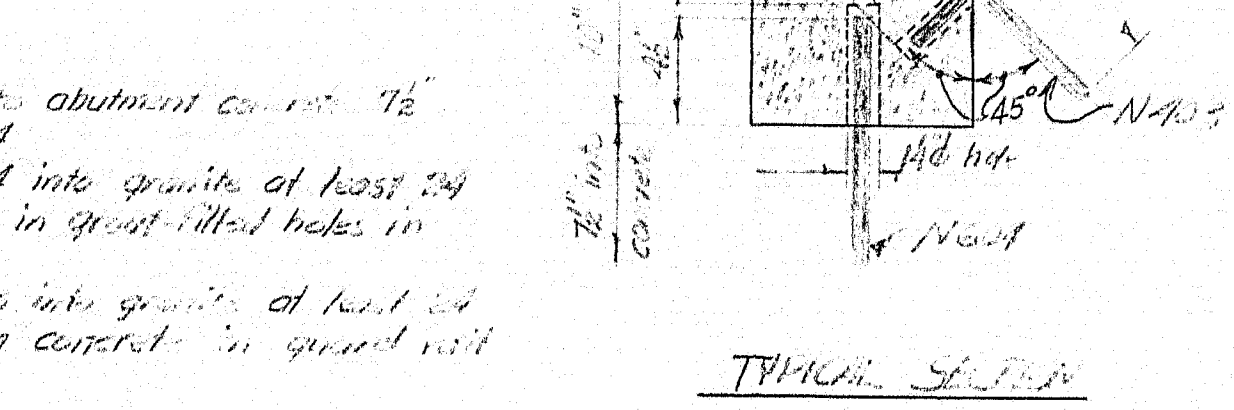
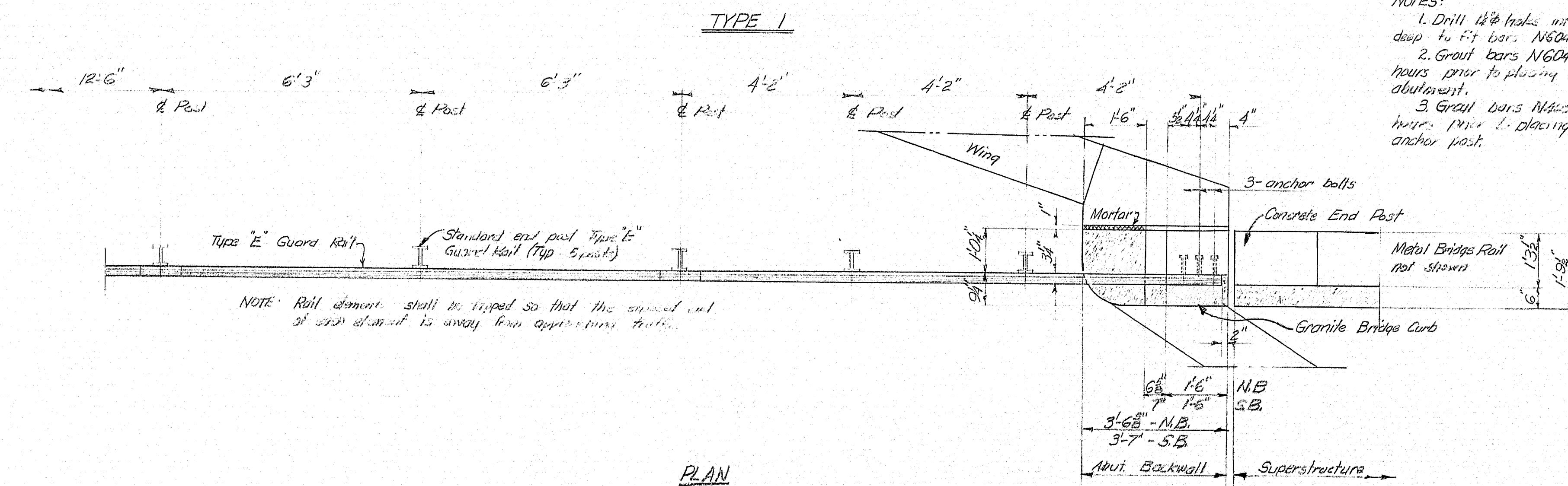
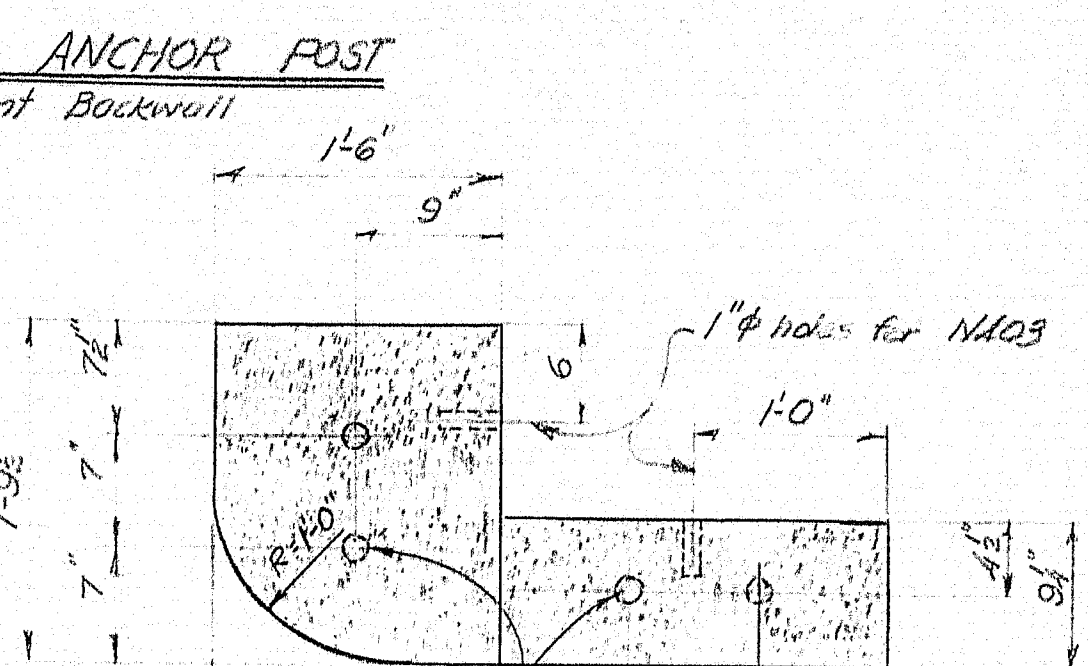
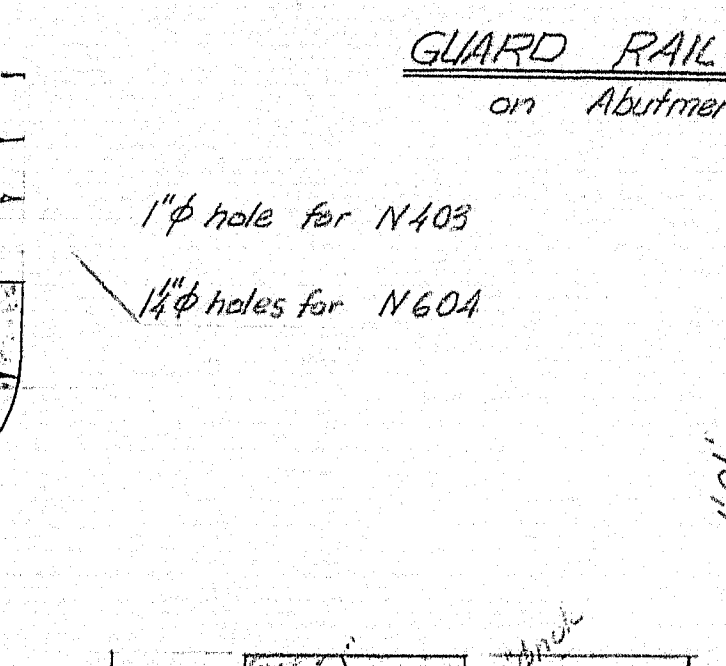
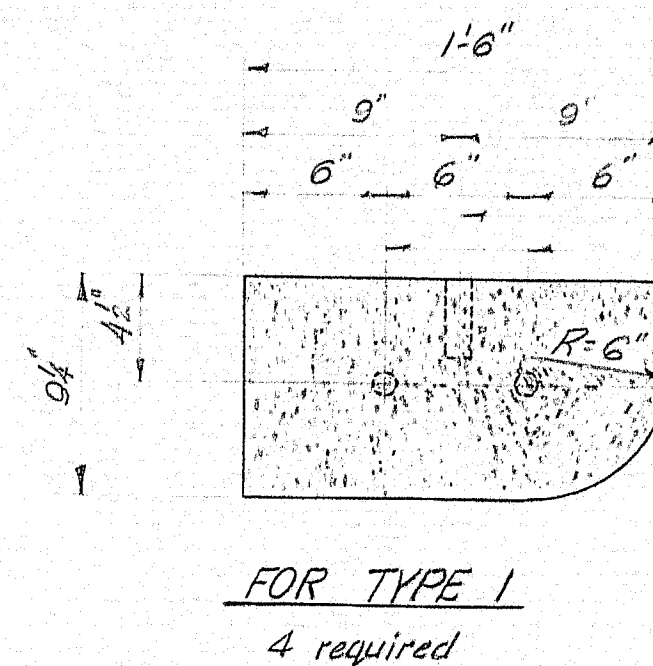
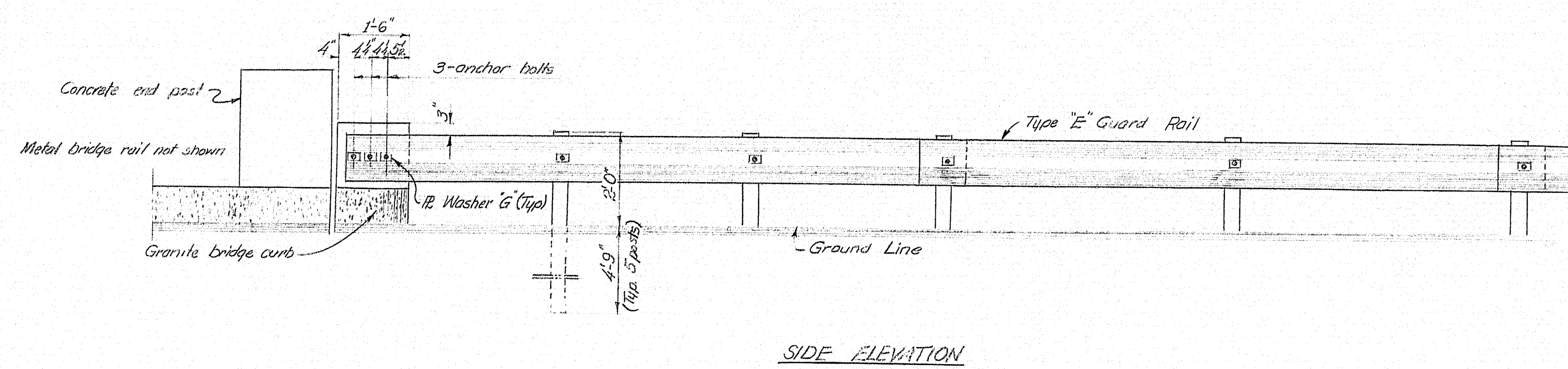
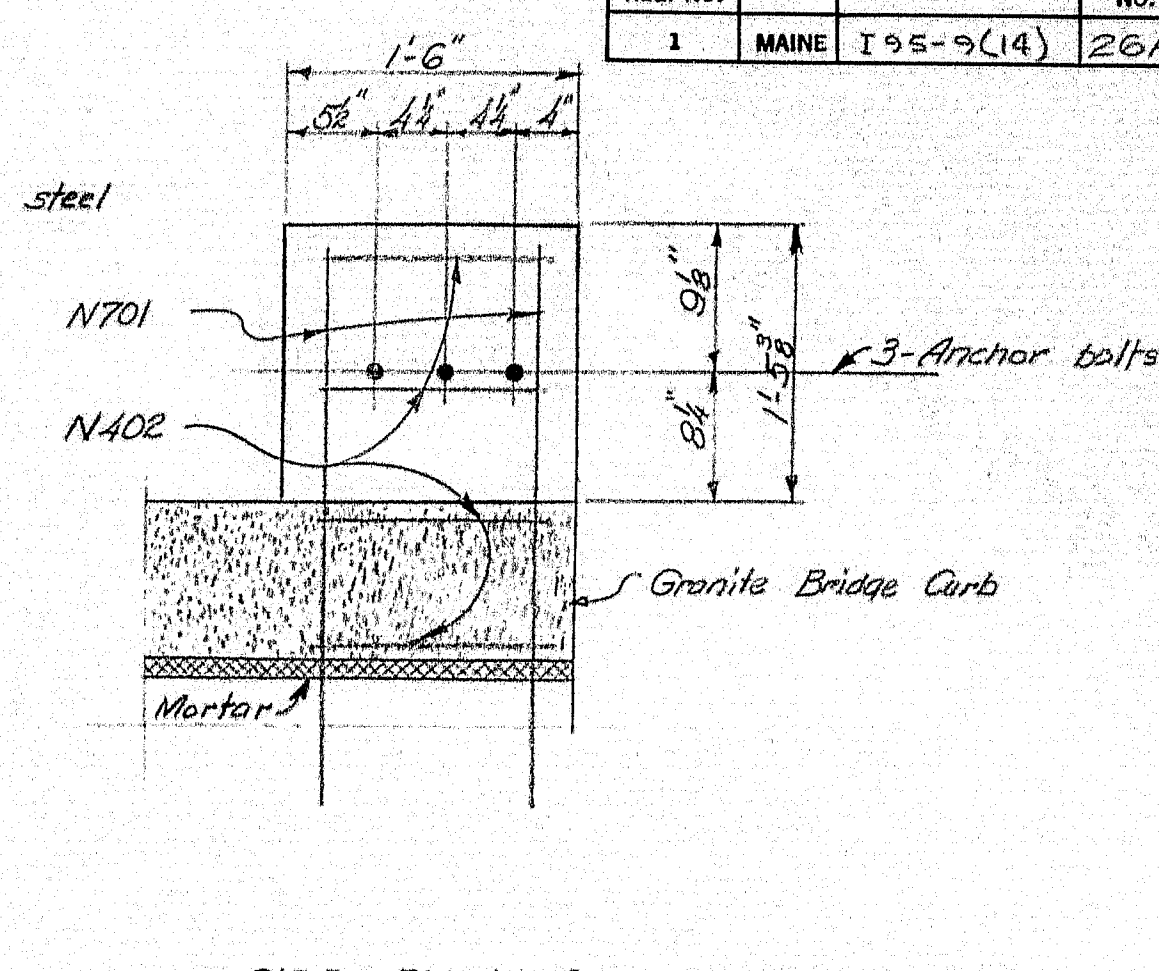
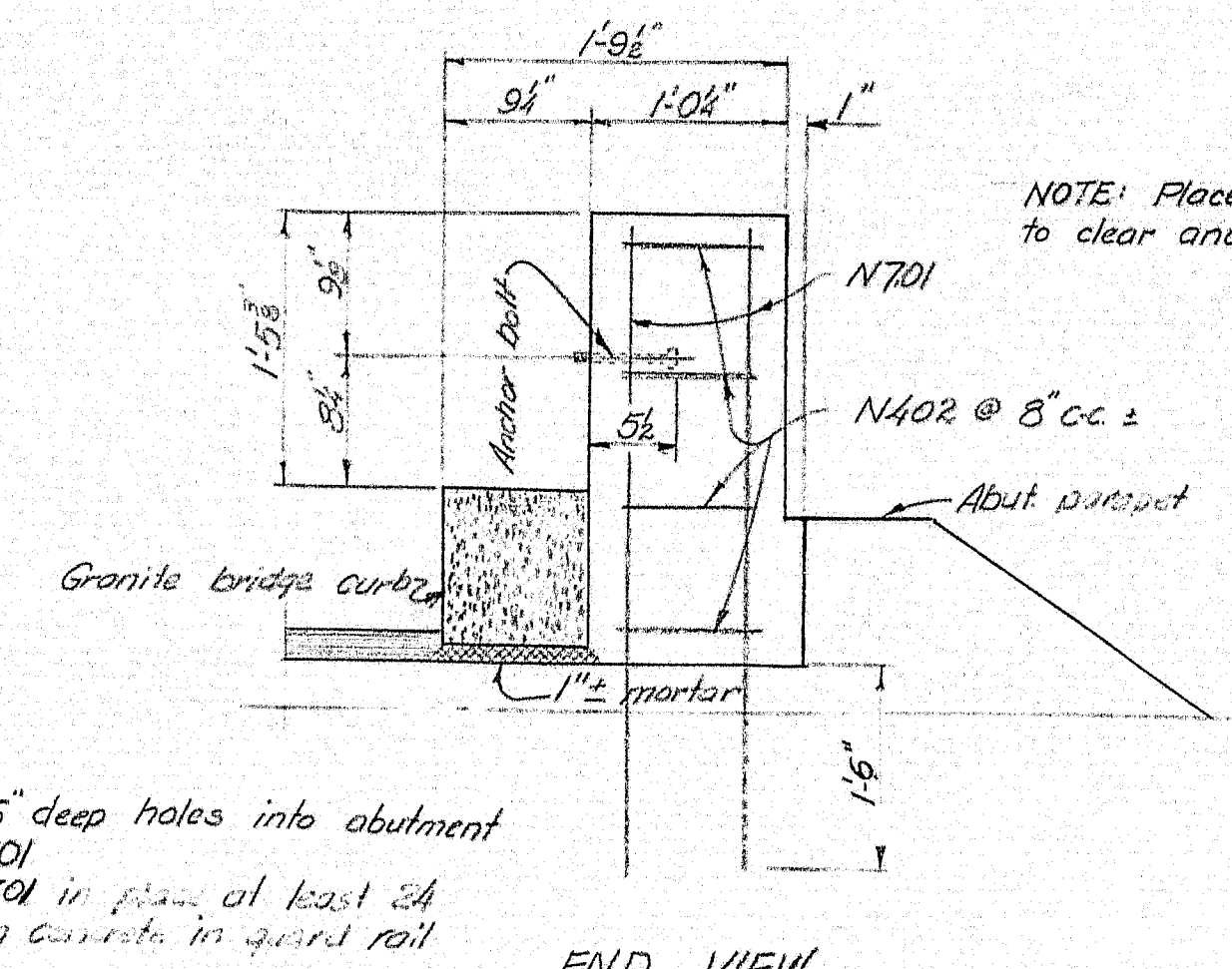
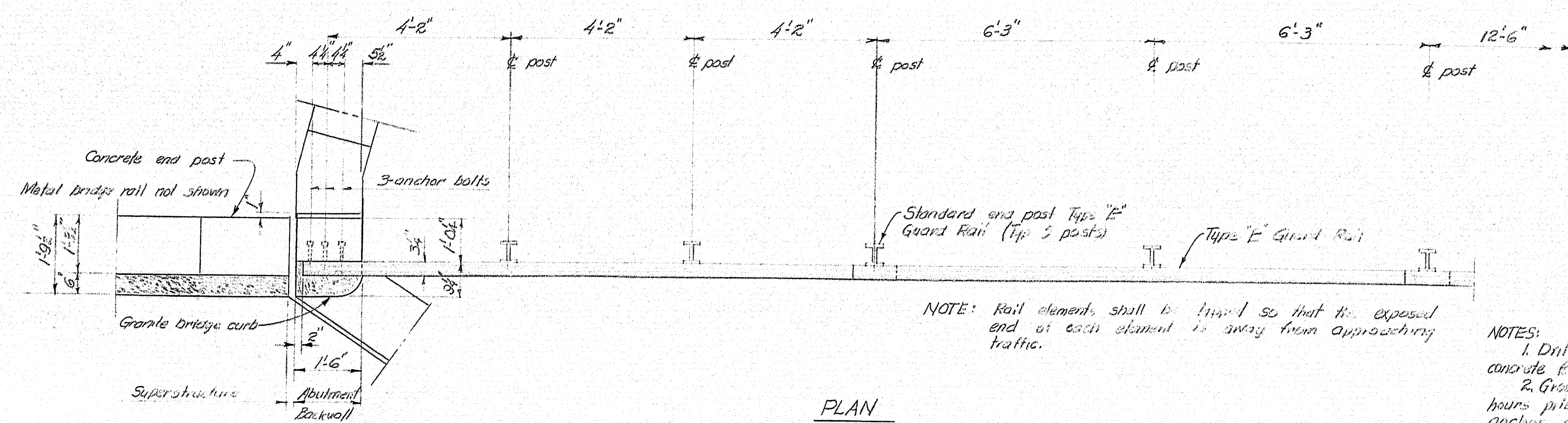


NOTES:
 Grout A671 bars into $1\frac{1}{2}"$ ϕ holes
 in stone prior to setting stone on
 backwall. Drill $1\frac{1}{2}"$ ϕ holes in backwall
 to suit A671 bars.
 Payment for drilling for and
 grouting of A671 bars to be included
 in the price for Item 705-14,
 Reinforcing Steel, Placing.
 Granite blocks shall be placed
 in position after or at the same
 time as curb on bridge is positioned.



M-7779 SMYRNA (14)

M-7779 SMYRNA (14)



REINFORCING STEEL

NOTE: Reinforcing steel shall be intermediate grade.

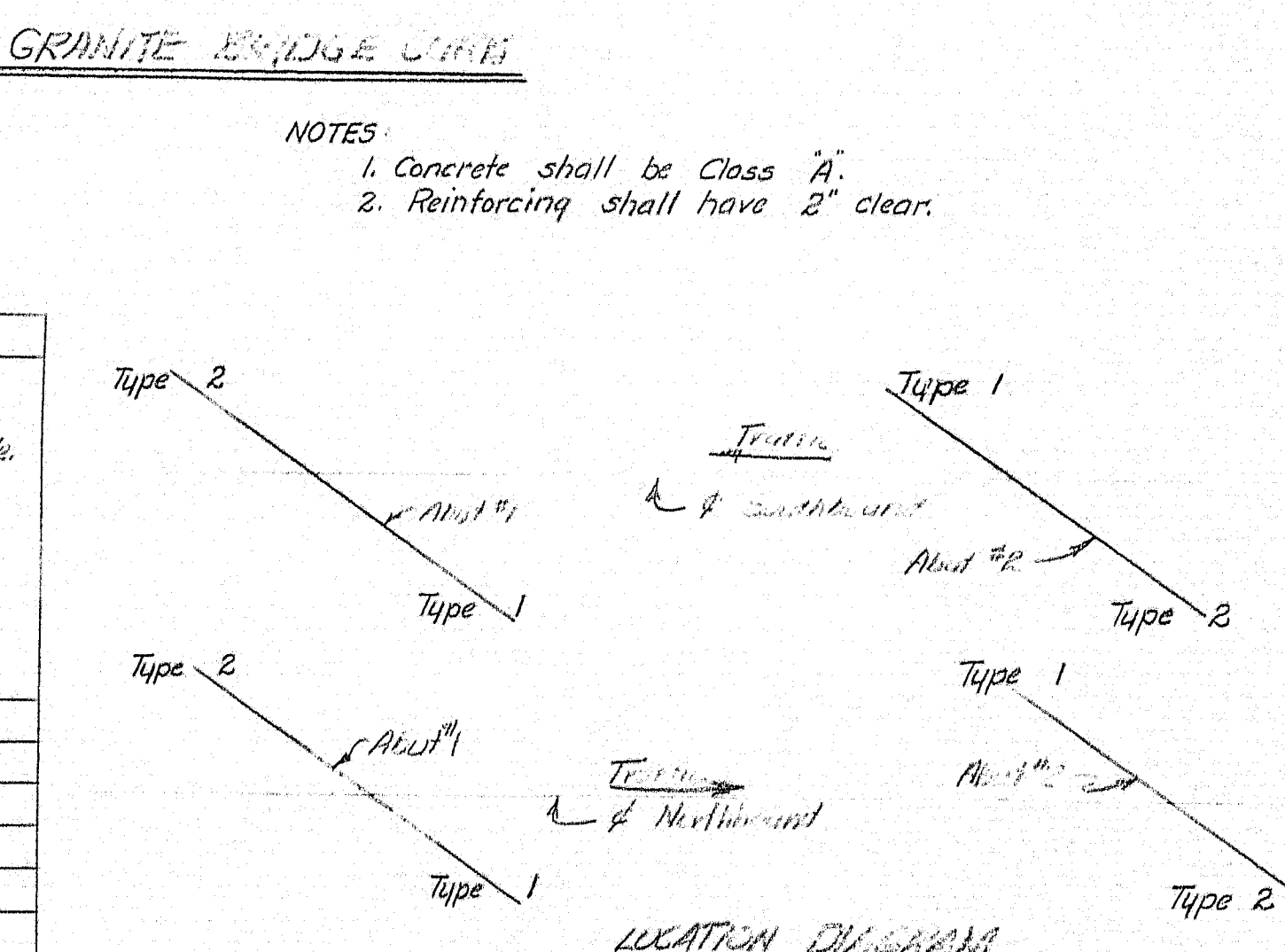
MARK	SIZE	NO.	LENGTH	LOCATION
N402	#4	32	4'-2"	Guard rail anchor post
N403	#4	12	6'-1"	Granite bridge curb
N701	#7	32	3'-8"	Guard rail anchor post
N604	#6	24	1'-0"	Granite bridge curb

STRAIGHT BAR

ANCHOR BOLT
2A required

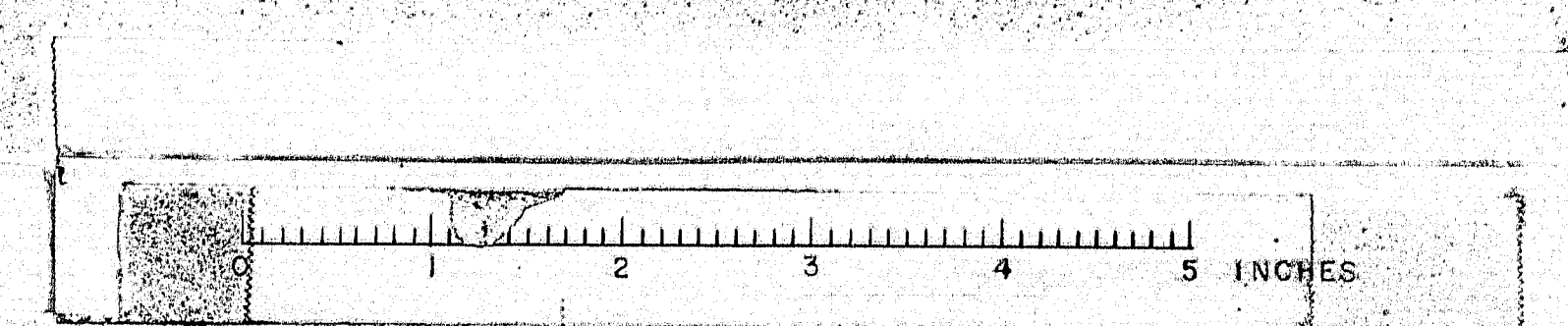
1/2" hole

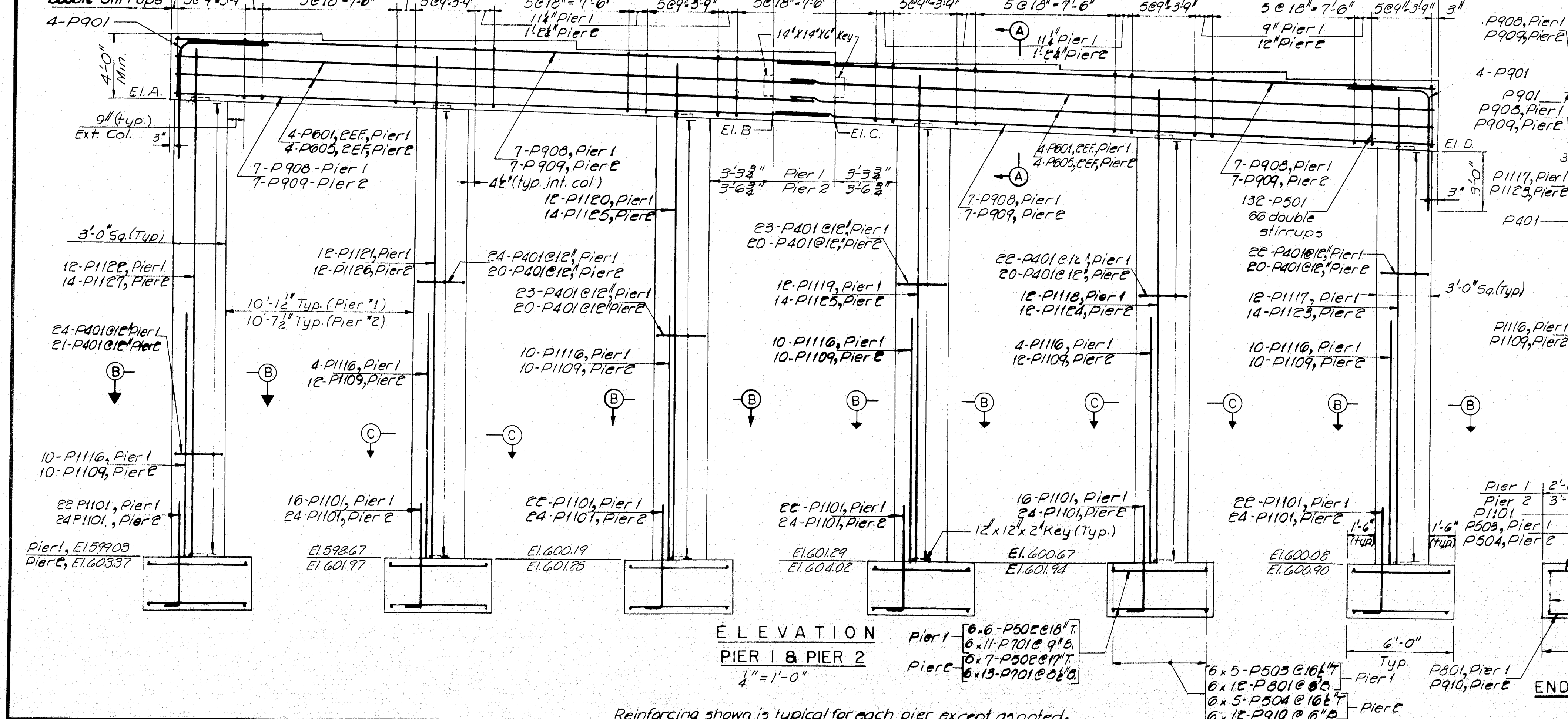
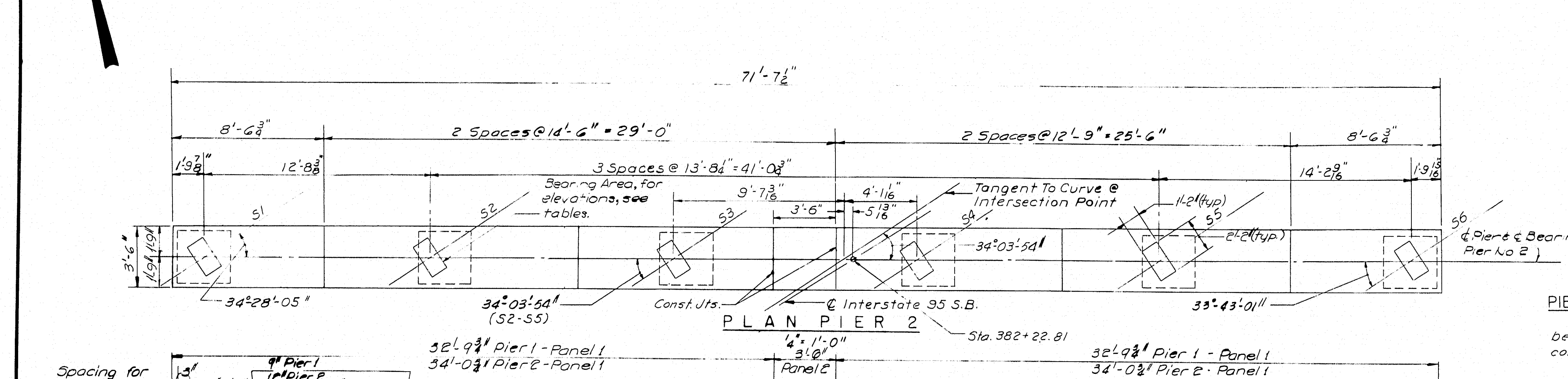
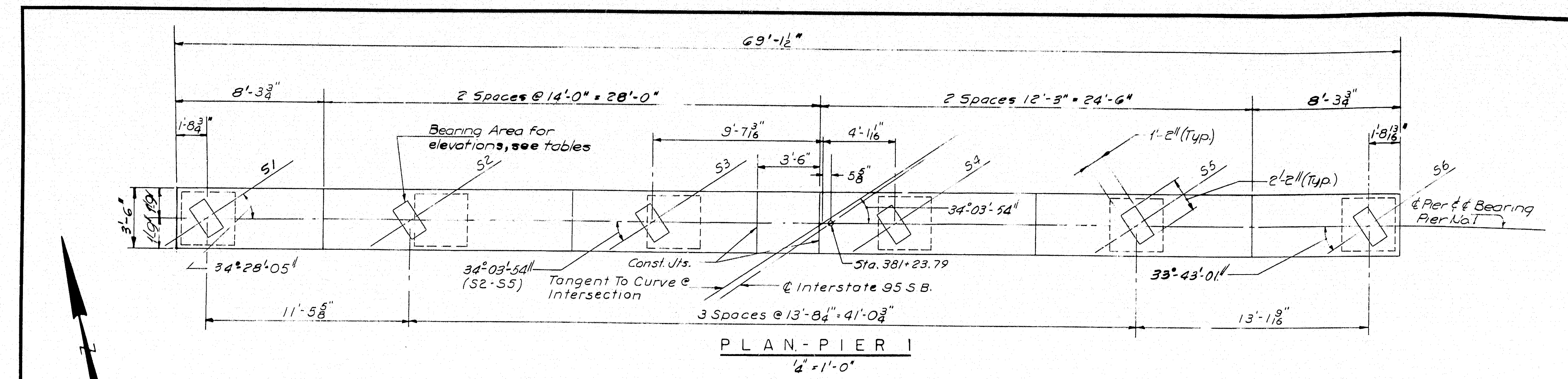
5" embedment in concrete



SHEET 8A OF 18 AUGUSTA, MAINE APRIL 1966

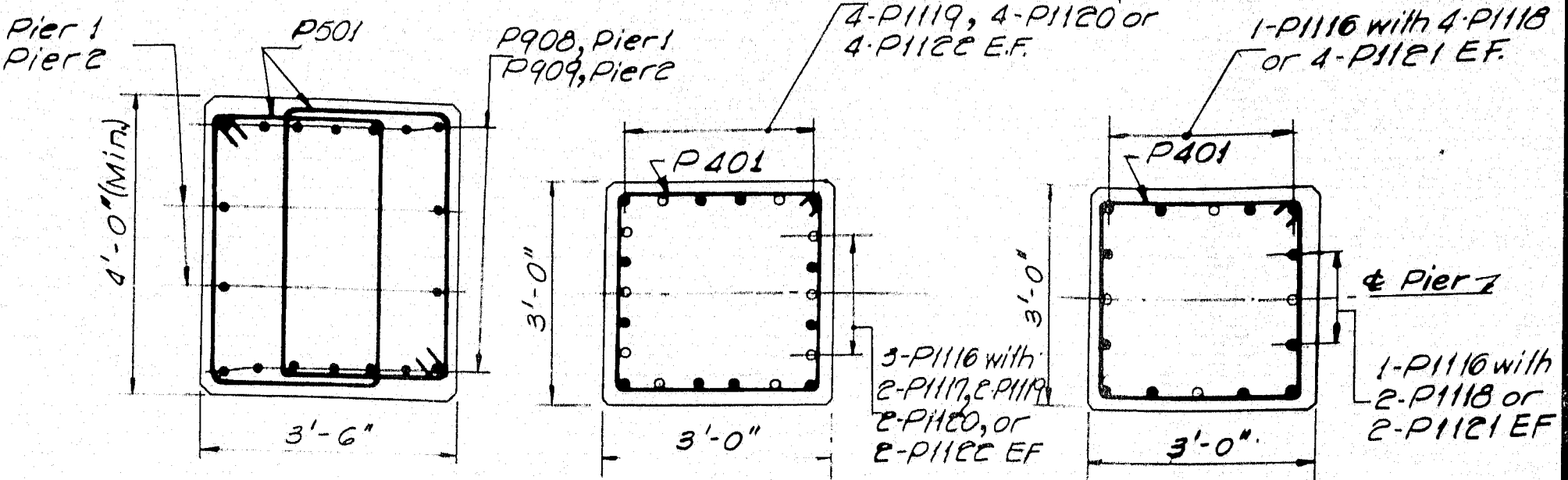
M-2230



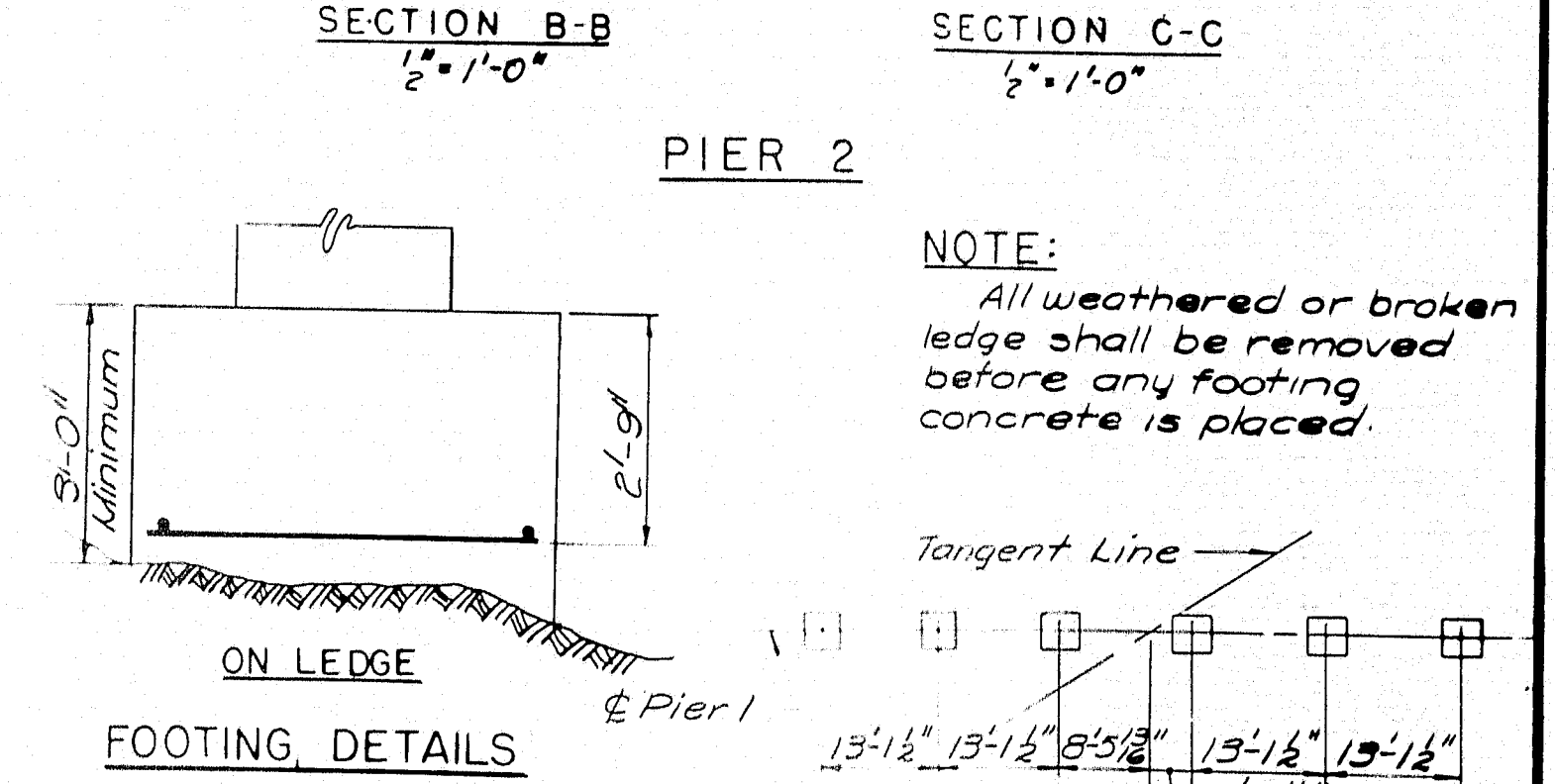


BEAM	PIER 1	PIER 2
S 1	626.78	625.38
S 2	626.41	624.97
S 3	625.97	624.53
S 4	625.59	624.10
S 5	625.09	623.66
S 6	624.66	623.20

PIER ELEVATIONS	A	B	C	D
PIER 1	622.62	621.53	621.41	620.32
PIER 2	621.17	620.08	619.96	618.87



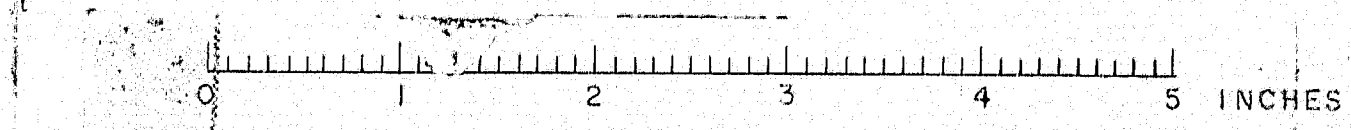
PIER CAP PLACING SEQUENCE
Place Panels 1 first. Panel 2 shall be placed not less than 30 days after completion of Panels 1.

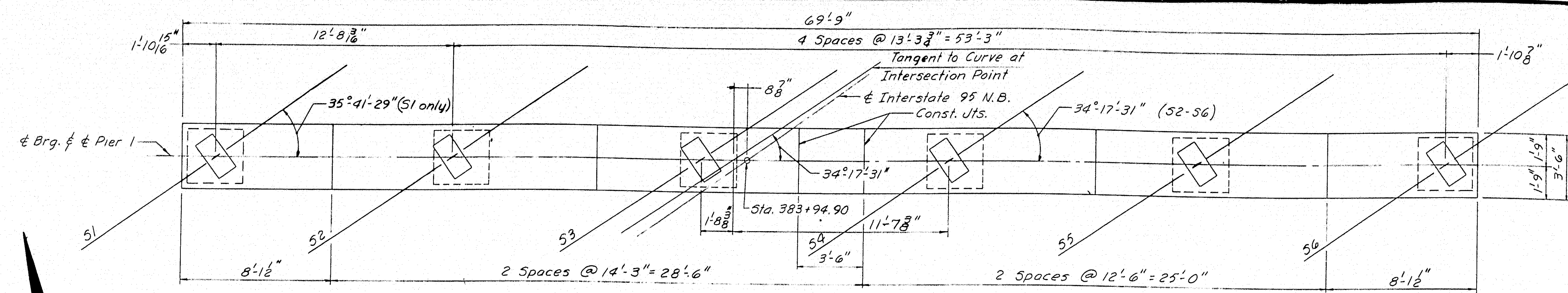


NOTES:
1. Dress bearing areas 1" larger all around than masonry plates to exact elevations shown.
2. Reinforcing steel to have 2" minimum cover, unless otherwise shown, except 3" cover for footing.
3. Maximum Footing Pressures:
Pier No. 1
Group I - 5.4 Tons/sq ft
Group II - 5.4 Tons/sq ft
Pier No. 2
Group I - 4.1 Tons/sq ft
Group II - 4.1 Tons/sq ft
4. et. denotes each face.
5. Top of footing elevation may be altered to suit field conditions. No change in top of footing elevations greater than two feet shall be made without approval of the Consulting Engineer.
6. Place reinforcing to clear anchor bolts.

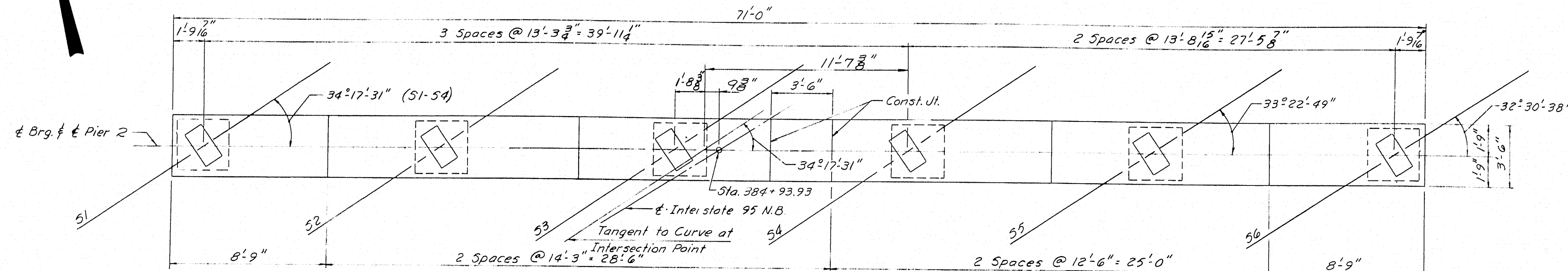
DESIGN - E.F.K. DETAIL - J.R.A.
TRACE -
CHECK - G.J.D.
BRIDGE NO. SURVEY -
PLOT -
STATE HIGHWAY COMMISSION
BRIDGE DIVISION
INTERSTATE 95 SB
OVER
U.S. ROUTE 2
IN THE TOWN OF
SMYRNA
ARROOSTOOK COUNTY
PIERS
HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
NEW YORK BOSTON KANSAS CITY
SHEET 9 OF 18 AUGUSTA, MAINE MARCH 1965

Reinforcing shown is typical for each pier except as noted.

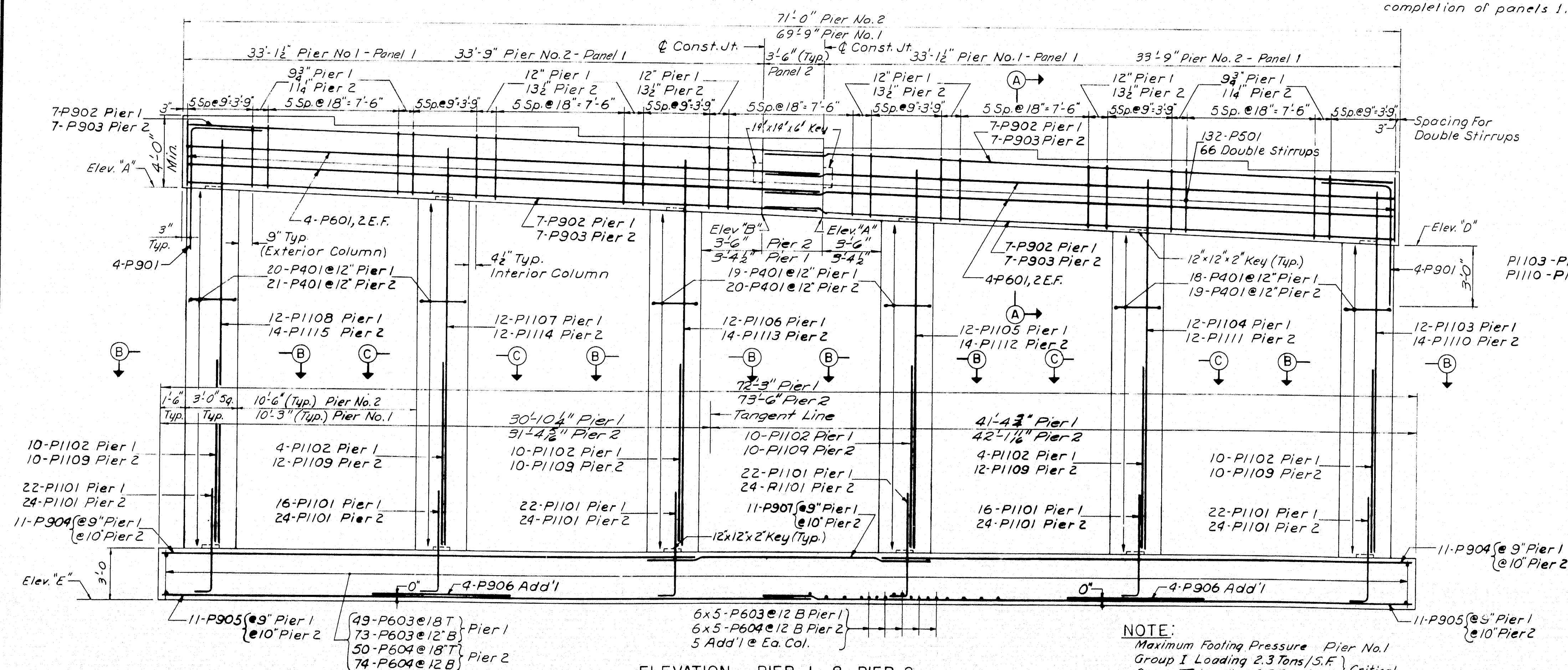




PLAN - PIER
1" = 1'-0"



PLAN - PIER 2
1/4" = 1'-0"



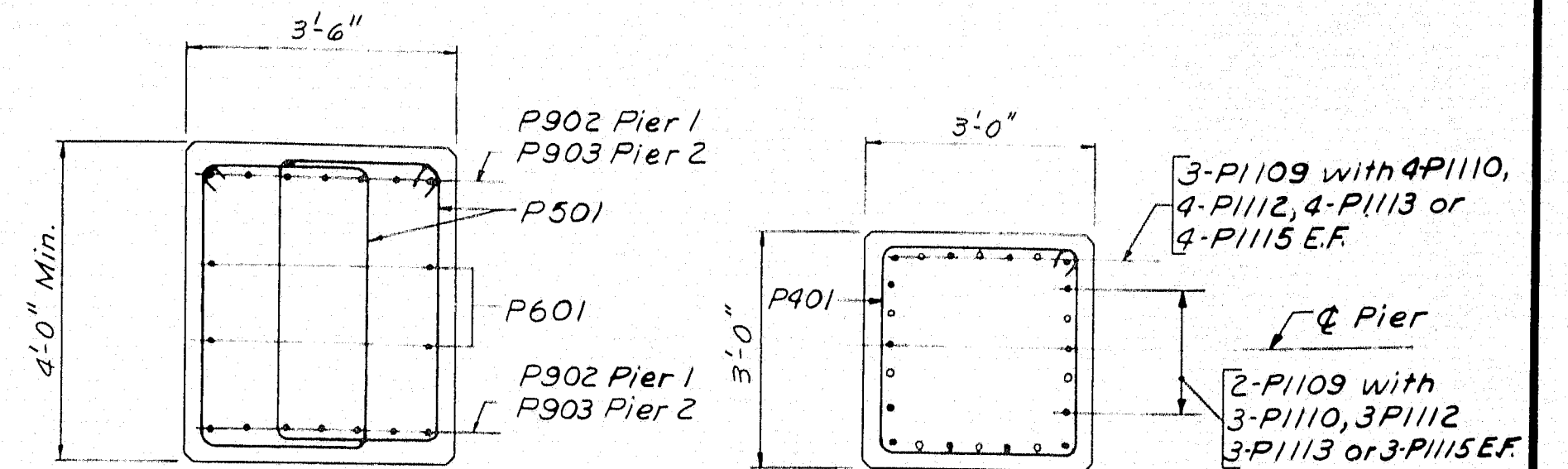
ELEVATION PIER 1 & PIER 2
4' = 1'-0"

Reinf. Shown Is Typ. For Each
Pier Except As Noted

NOTE:
Maximum Footing Pressure Pier No.1
Group I Loading 2.3 Tons/S.F. } Critical
Group III Loading 2.5 Tons/S.F. }
Maximum Footing Pressure Pier No.2
Group I Loading 2.0 Tons/S.F. } Critical
Group III Loading 2.4 Tons/S.F. }
For other notes see sheet 9

BEARING ELEVATIONS						
PIER	S1	S2	S3	S4	S5	S6
1	624.07	623.61	623.12	622.63	622.13	621.63
2	622.98	622.47	621.95	621.44	620.90	620.35

PIER	A	B	C	D	E
1	619.87	618.64	618.51	617.29	596.50
2	618.79	617.45	617.31	615.97	594.90



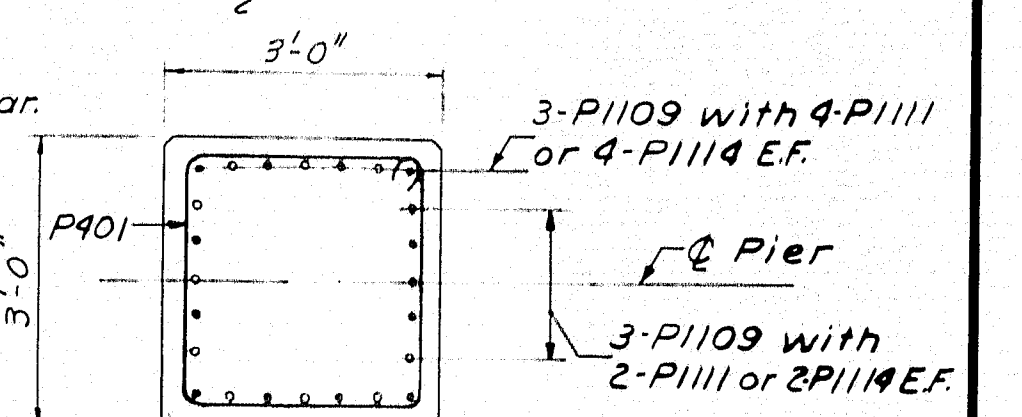
PIER CAP PLACING SEQUENCE
Place Panels 1 first. Panel 2 shall be placed not less than 30 days after completion of panels 1.

SECTION A-A
1" = 1'-0"

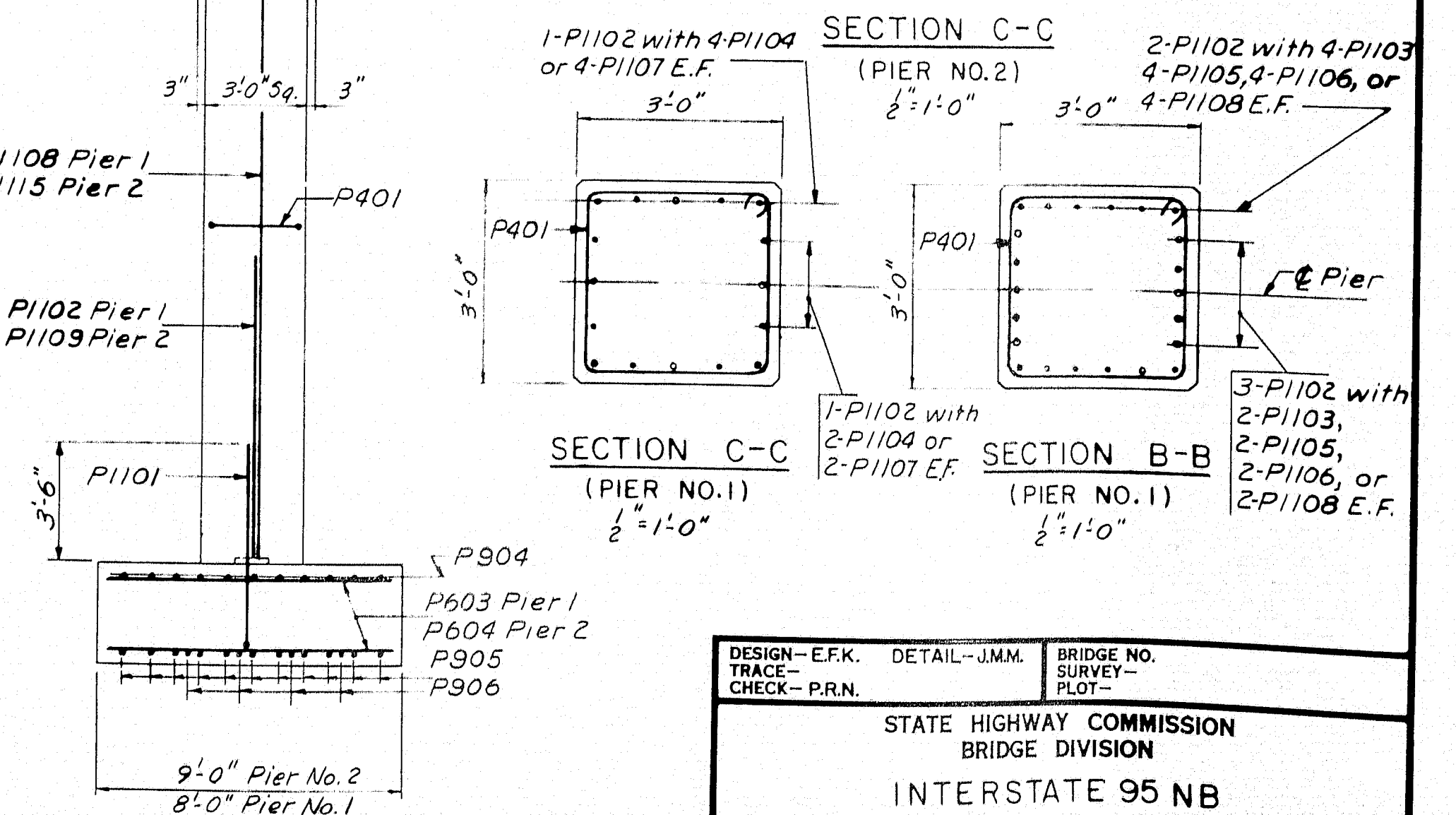
LEGEND

- Indicates full length bar.
- Indicates cut-off bar.

SECTION B-B
(PIER NO.2)
1'-11.0"



SECTION C-C
(PIER NO.2)



END ELEVATION
4" = 1'-0"

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS

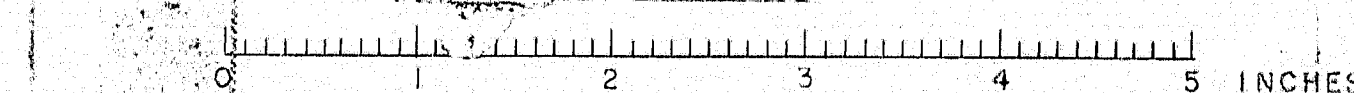
NEW YORK

POST

KANSAS CITY

DESIGN—E.F.K. TRACE— CHECK—P.R.N.	DETAIL—J.M.M.	BRIDGE NO. SURVEY— PLOT—
STATE HIGHWAY COMMISSION BRIDGE DIVISION INTERSTATE 95 NB OVER U.S. ROUTE 2 IN THE TOWN OF SMYRNA ARROSTOOK COUNTY PIERS SHEET 10 OF 18 AUGUSTA, MAINE MARCH 1965		

M-2227 SMYRNA (14)



PEDESTALS
12-EPC2 Required
6-EPC0 Required
6-FPC4 Required

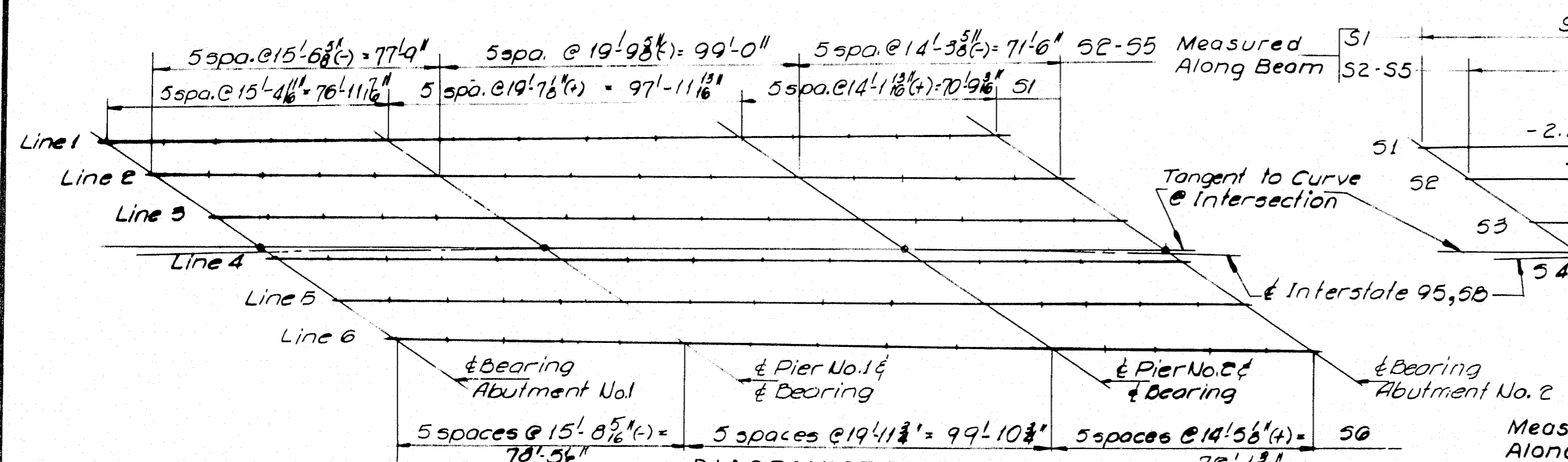
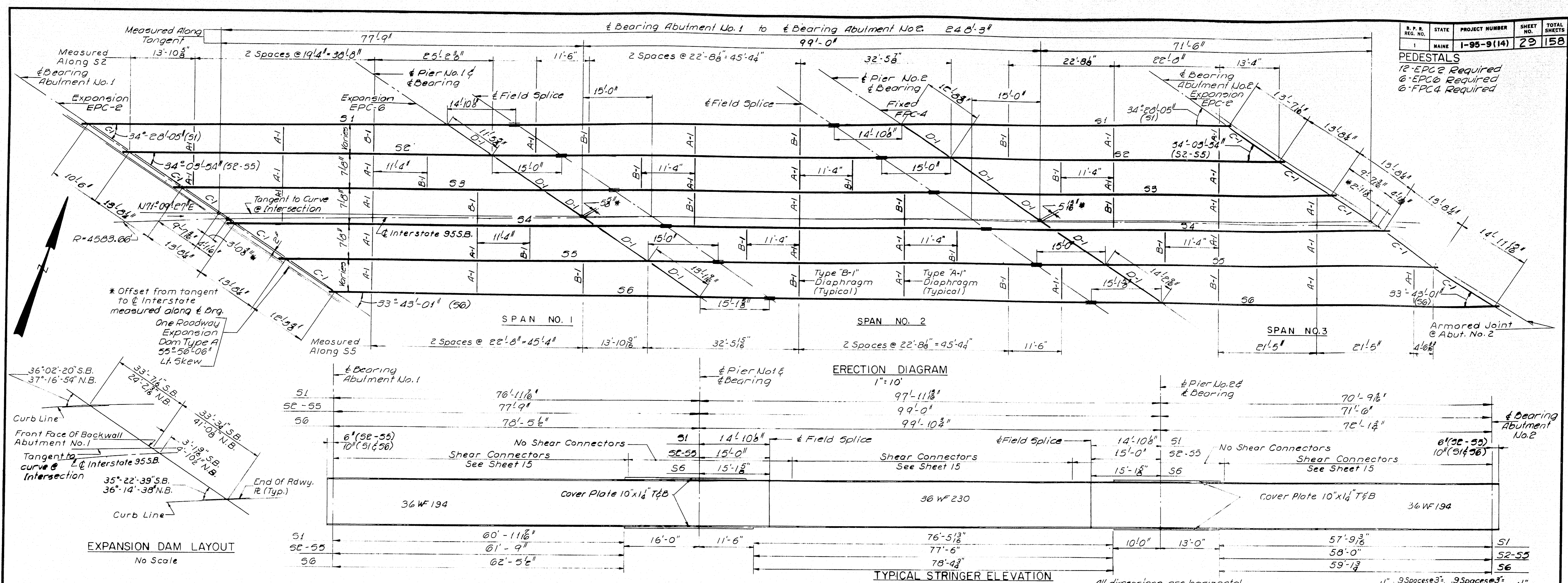
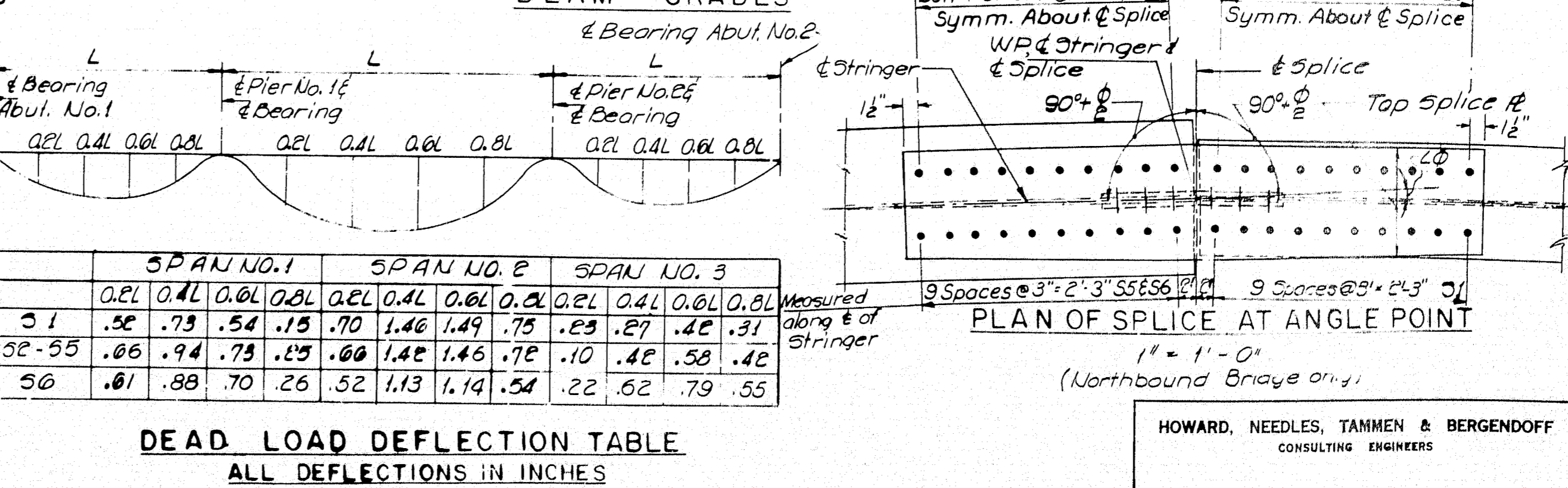


DIAGRAM OF BLOCKING POINTS

BOTTOM OF SLAB ELEVATIONS AT BLOCKING POINTS

		SPAN NO. 1						SPAN NO. 2						SPAN NO. 3					
	to Brg. About No.1	15' 4 1/2"	30' 9 3/8"	46' 2 1/8"	61' 6 3/8"		to Pier No.1	19' 7 1/2"	39' 2 1/2"	58' 9 3/8"	78' 4 3/8"		to Pier No.2	14' 5 1/2"	28' 3 3/8"	42' 5 1/2"	56' 7 3/8"	to Brg. About No.2	
S-1																			
S-6		15' 8 3/4"	31' 4 3/8"	47' 0 3/8"	62' 9 3/8"			19' 11 3/8"	39' 4 1/2"	59' 1 1/8"	79' 1 1/8"			14' 5 1/2"	28' 10 3/8"	43' 3 7/8"	57' 8 3/8"		
S2-S5		15' 6 3/8"	31' 1 1/8"	46' 7 1/8"	61' 2 3/8"			19' 9 3/8"	39' 7 3/8"	59' 4 1/8"	79' 2 3/8"			14' 5 3/8"	28' 7 3/8"	42' 10 1/8"	57' 6 1/8"		
Line 1	633.07	632.78	632.48	632.16	631.82	631.50	631.15	630.81	630.42	629.98	629.55	629.24	628.89	628.49	628.10	627.73	627.35	626.98	
Line 2	632.73	632.45	632.15	631.81	631.46	631.18	630.77	630.42	630.03	629.58	629.14	628.70	628.27	627.81	627.34	626.87	626.44	626.01	
Line 3	632.28	632.00	631.70	631.37	631.02	630.69	630.33	629.95	629.59	629.14	628.70	628.24	627.81	627.37	626.90	626.44	625.97	625.51	
Line 4	631.84	631.56	631.26	630.93	630.58	630.24	629.89	629.54	629.15	628.71	628.27	627.83	627.39	626.95	626.48	626.04	625.57	625.10	
Line 5	631.40	631.12	630.82	630.49	630.14	629.79	629.45	629.10	628.71	628.27	627.83	627.39	626.95	626.50	626.03	625.58	625.14	624.67	
Line 6	631.00	630.71	630.40	630.07	629.72	629.35	628.97	628.64	628.24	627.80	627.37	626.91	626.45	625.98	625.50	625.03	624.56	624.09	



NOTE:
For additional Splice Details, see Standard Details BD 103-64

SPICE DETAIL
1'-0"

NOTES:
1. For Specifications and References see Sheet 12
2. For Blocking Detail see Sheet 12
3. No shop camber required.
4. Natural mill camber to be placed up.

DESIGN - E.F.K. DETAIL DAT
TRACE -
CHECK - P.R.N.

BRIDGE NO. SURVEY -
PLOT -

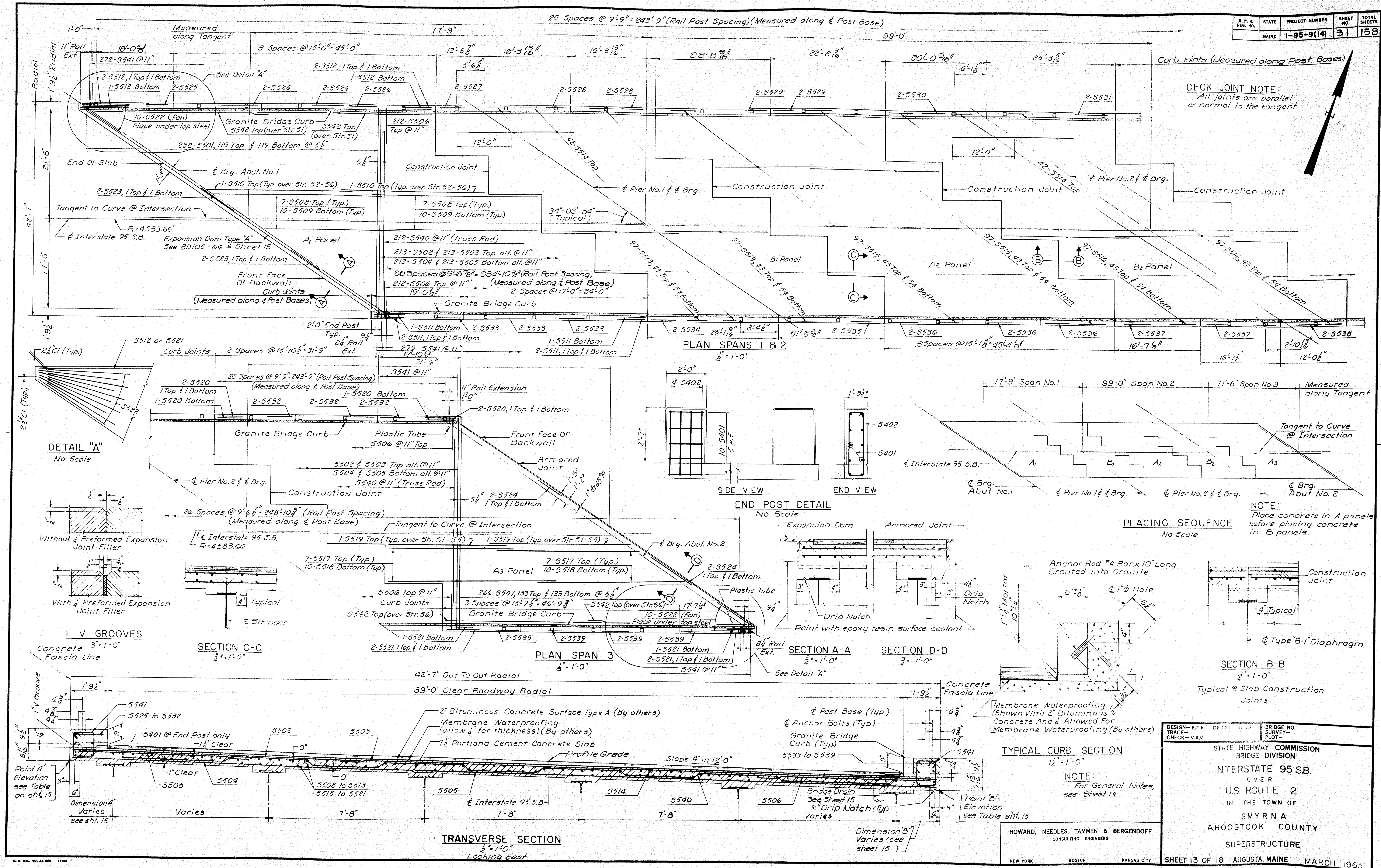
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

INTERSTATE 95 SB
OVER
U.S. ROUTE 2
IN THE TOWN OF
SMYRNA
AROSTOOK COUNTY
STRUCTURAL STEEL & BLOCKING

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
NEW YORK BOSTON KANSAS CITY

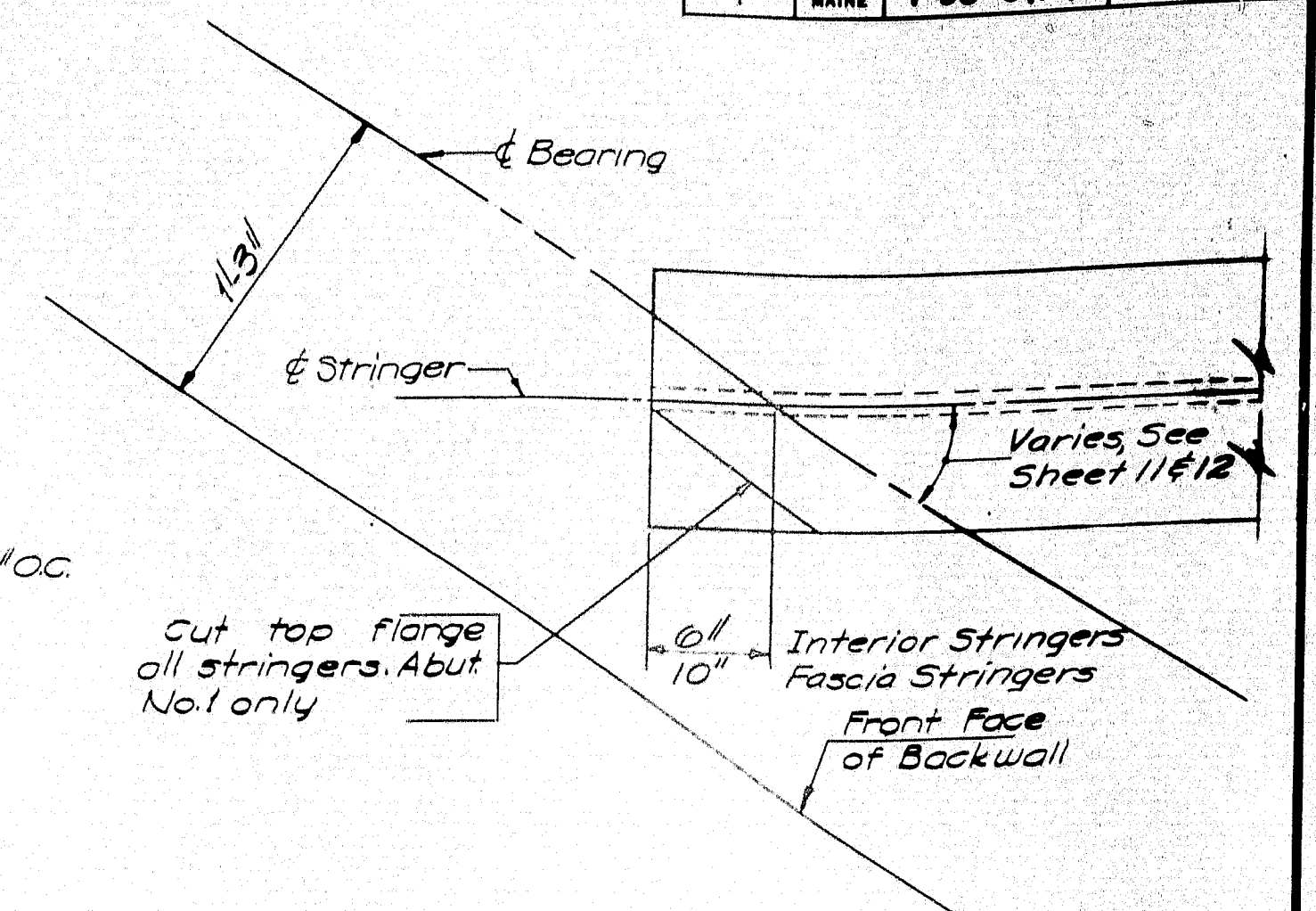
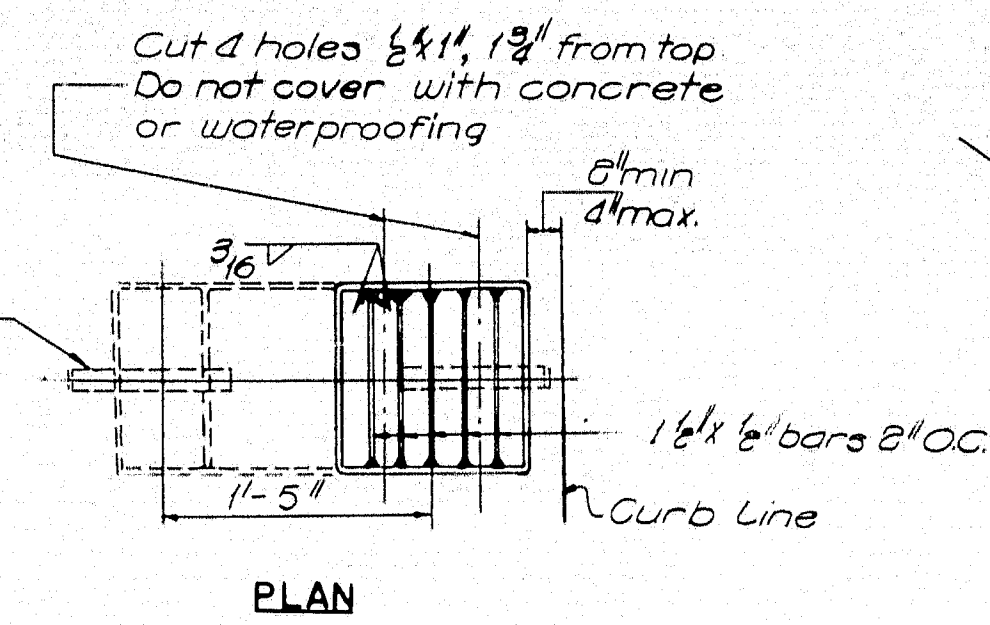
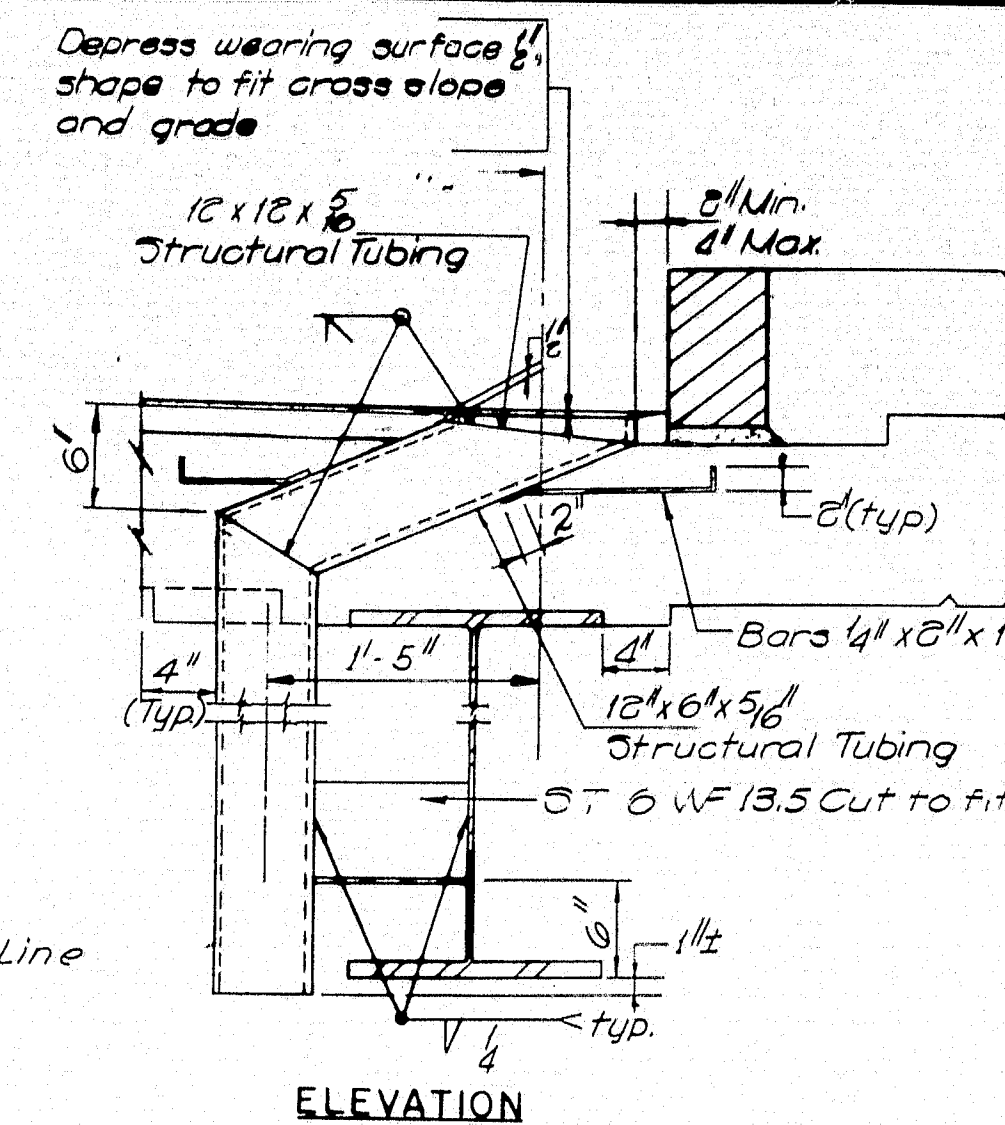
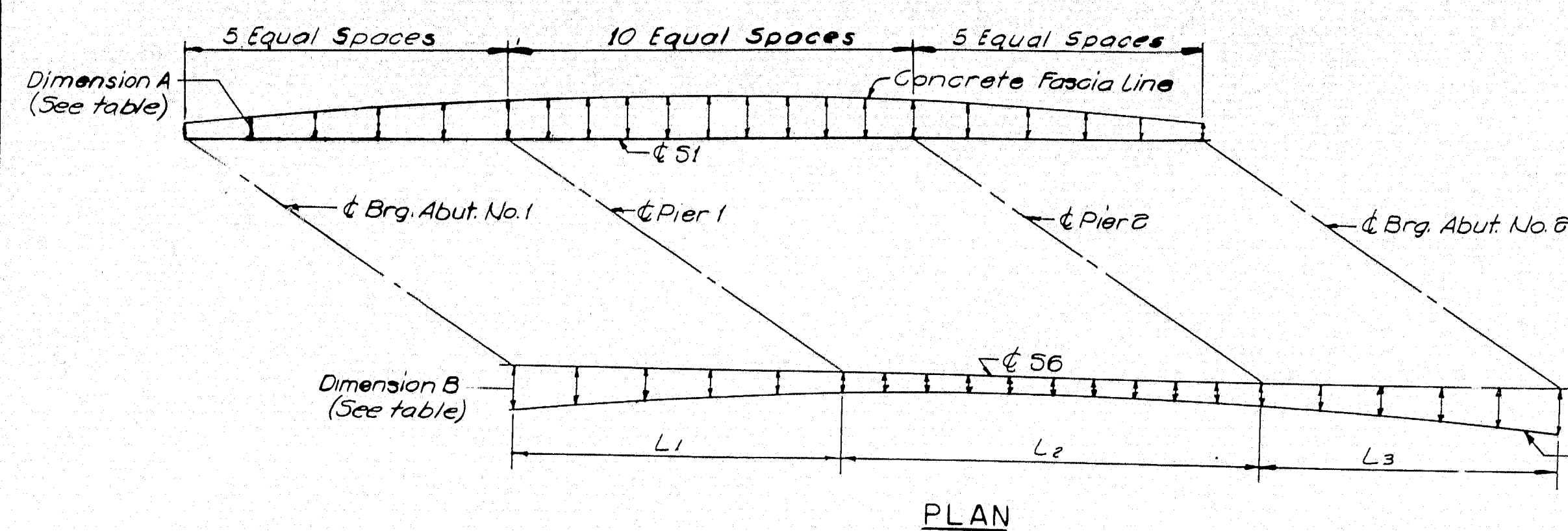
SHEET 11 OF 18 AUGUSTA, MAINE MARCH 1965

R. P. R. REG. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	1-95-9(14)	31	158



M-2235 SMYRNA (14)

SHEET 13 OF 18 AUGUSTA, MAINE MARCH 1965



BRIDGE DRAIN NOTES

1. Two bridge drains south side Span 1 & Span 3.
2. Drains shall be placed so they are at least 10'-0" from piers. Exact position to be determined in field.
3. For approximate location of drains, see Sheet 2.

BRIDGE DRAIN TYPE B
11-1104

EXPANSION DAM DIMENSIONS									
IK	L	G	H	K ₁ 45°	V ₁ 45°	M	N	P	Q
1'-2"	8'5"	2'3"	3'5"	1'-4"	2'6"	10"	4"	1'6"	25

For location of dimensions see Standard Detail BD 105-64

END OF BEAM DETAIL
161-1401

	LOC.	2 STUDS 5' x 7/8"	SPIRALS 5' x 5/8"
a	21°44' - 7°10'	D17°53' - 7°9'	
b	19°55' - 7°11'	D15°56' - 8°14'	
c	14°56½' - 7°7"	D12°8' - 8°0"	
d	12°52' - 5°10'	S19°55' - 7°11'	
e	11°03' - 5°13'	S17°53' - 7°9½'	
f	13°07' - 7°7"	S19°44' - 8°1½'	
g	17°5½' - 7°9½'	D14°7' - 8°12'	
h	19°55' - 7°11'	D16°56' - 8°0"	
i	19°16'	15°16'	
j	24°55' - 10°10'	D19°56' - 10°3½'	
k	20°56' - 10°10'	S31°04' - 10°4'	
l	15°08' - 10°10'	S25°55' - 10°5'	
m	15°08' - 10°10'	S25°55' - 10°5'	
n	20°56' - 10°10'	S31°04' - 10°4'	
o	24°55' - 10°10'	D19°56' - 10°3½'	
p	17°05' - 7°11'	D15°56' - 7°6'	
q	16°05½' - 7°4"	S19°47' - 7°7'	
r	12°7' - 7°0"	D12°44½' - 7°1½'	
s	9°09' - 6°9"	S16°55' - 7°4'	
t	11°08' - 7°4"	S18°05' - 7°6'	
u	13°06½' - 7°10½'	D11°08' - 7°4'	
v	10°05' - 7°6"	D16°56' - 7°7'	
w	20°54' - 7°16"	D19°53½' - 7°4'	

* Place 1
to Beam &

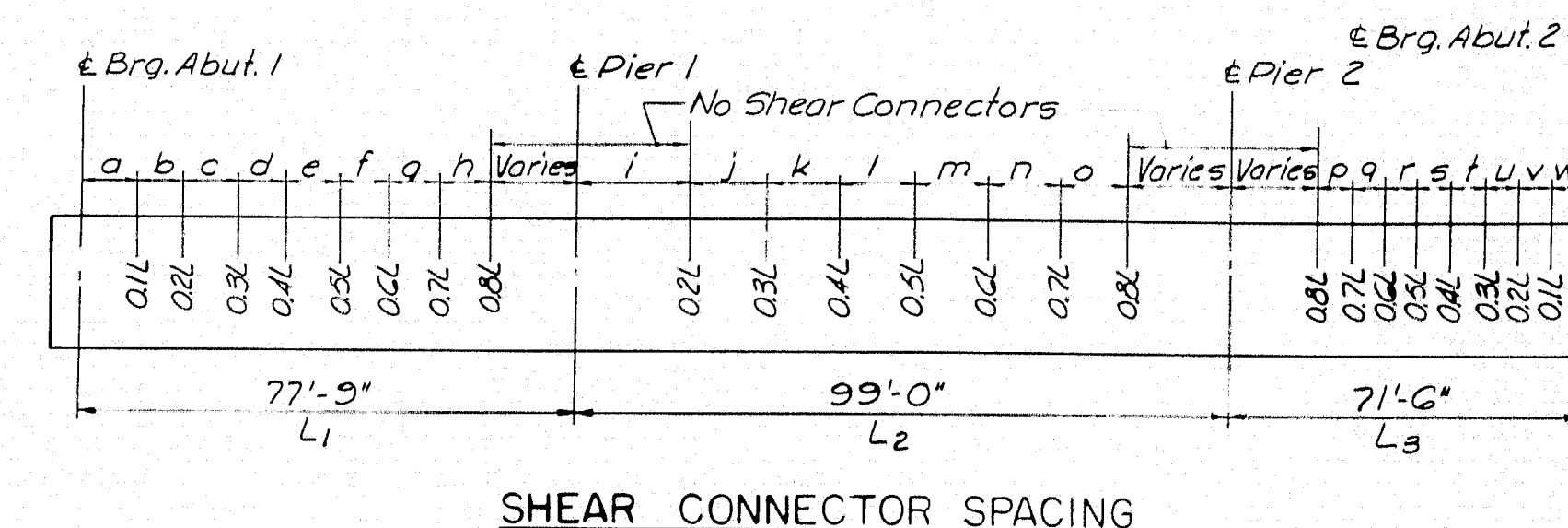
		CONCRETE										FASCIA					TIES									
		CBG	084	041	061	081	Per1	016	084	036	046	036	046	036	061	076	084	094	Per8	084	041	061	084	CBG		
Dim. A	3.8	11.91	84.12	84.53	81.91	81.13	34.21	31.91	33.53	34.23	34.23	34.23	34.23	34.23	34.23	34.23	31.91	31.91	34.23	84.12	84.53	81.91	81.13	11.91		
Dim. B	3.8	31.74	32.81	31.93	34.61	34.31	31.09	11.14	14.03	14.93	14.93	14.93	14.93	14.93	14.93	14.93	31.91	31.91	34.23	34.61	34.61	34.61	34.61	31.74		
Dim. A	1.8	11.91	84.12	81.93	81.74	81.83	81.94	81.94	84.10	81.94	81.94	81.94	81.94	81.94	81.94	81.94	81.94	81.94	81.94	81.94	81.94	81.94	81.94	11.91		
Dim. B	1.8	34.23	34.23	34.53	34.21	11.13	11.93	11.91	14.03	14.03	14.03	14.03	14.03	14.03	14.03	14.03	31.91	31.91	34.23	34.23	34.23	34.23	34.23	34.23		
Point A Elev. 3.8	033.10	038.84	038.55	038.23	031.90	031.59	031.41	031.64	031.08	030.90	030.72	030.52	030.30	030.08	030.08	030.08	034.73	034.73	034.73	034.73	034.73	034.73	034.73	033.10		
Point B Elev. 3.8	030.97	030.62	030.33	030.00	029.06	029.59	029.23	028.95	028.78	028.59	028.40	028.19	027.98	027.75	027.53	027.30	027.09	026.87	026.65	026.43	026.21	025.99	025.77	030.97		
Point A Elev. 1.8	029.93	029.72	029.59	029.38	029.14	028.91	028.77	028.65	028.53	028.39	028.24	028.09	027.89	027.69	027.49	027.29	027.09	026.87	026.65	026.43	026.21	025.99	025.77	029.93		
Point B Elev. 1.8	027.49	027.31	027.09	026.84	026.56	026.29	026.13	025.97	025.82	025.66	025.48	025.28	025.07	024.86	024.64	024.41	024.15	023.87	023.56	023.21	022.81	022.39	021.91	027.49		
NOTE																										

NOTE

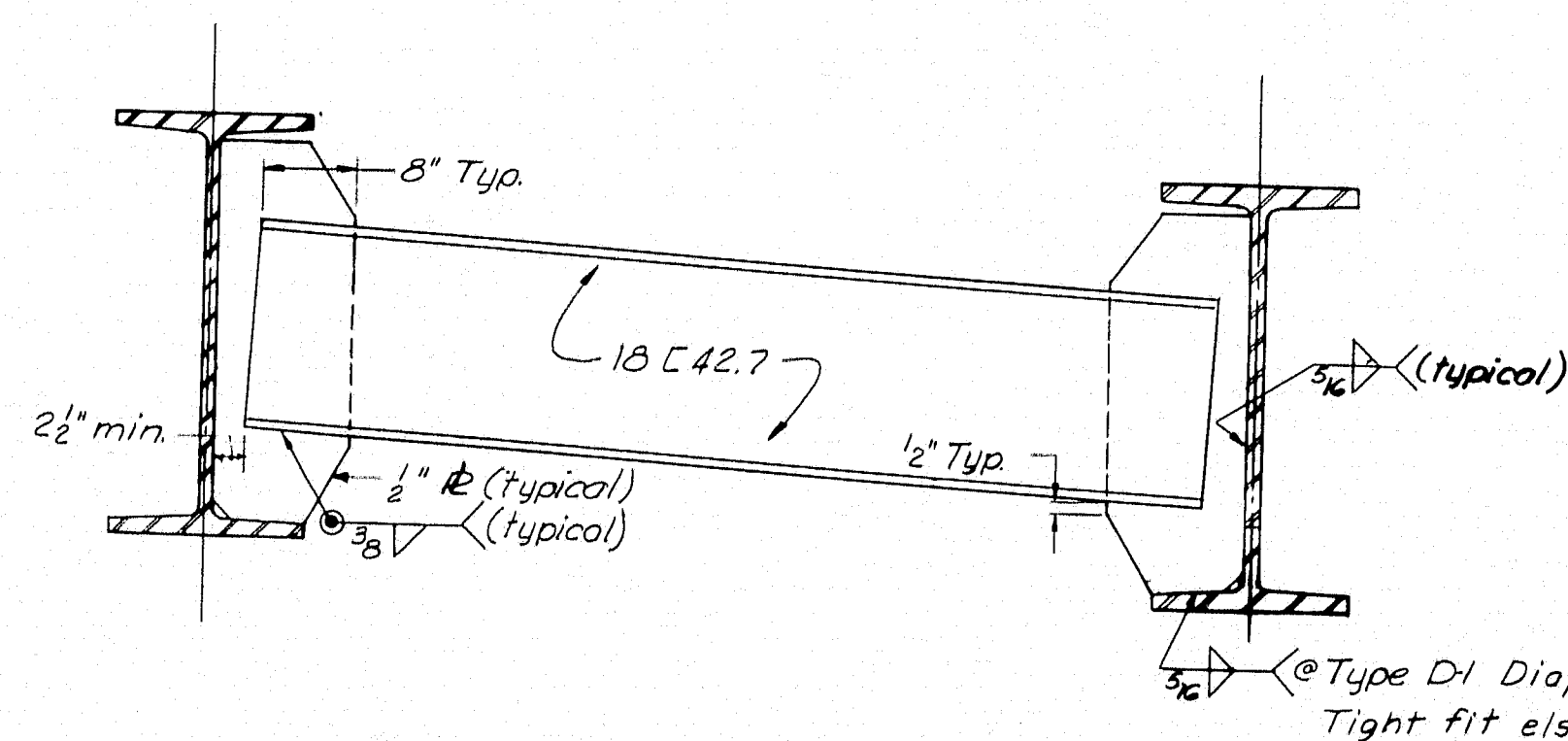
For location of Points A & B see Sheet 13 of 14

NOTE

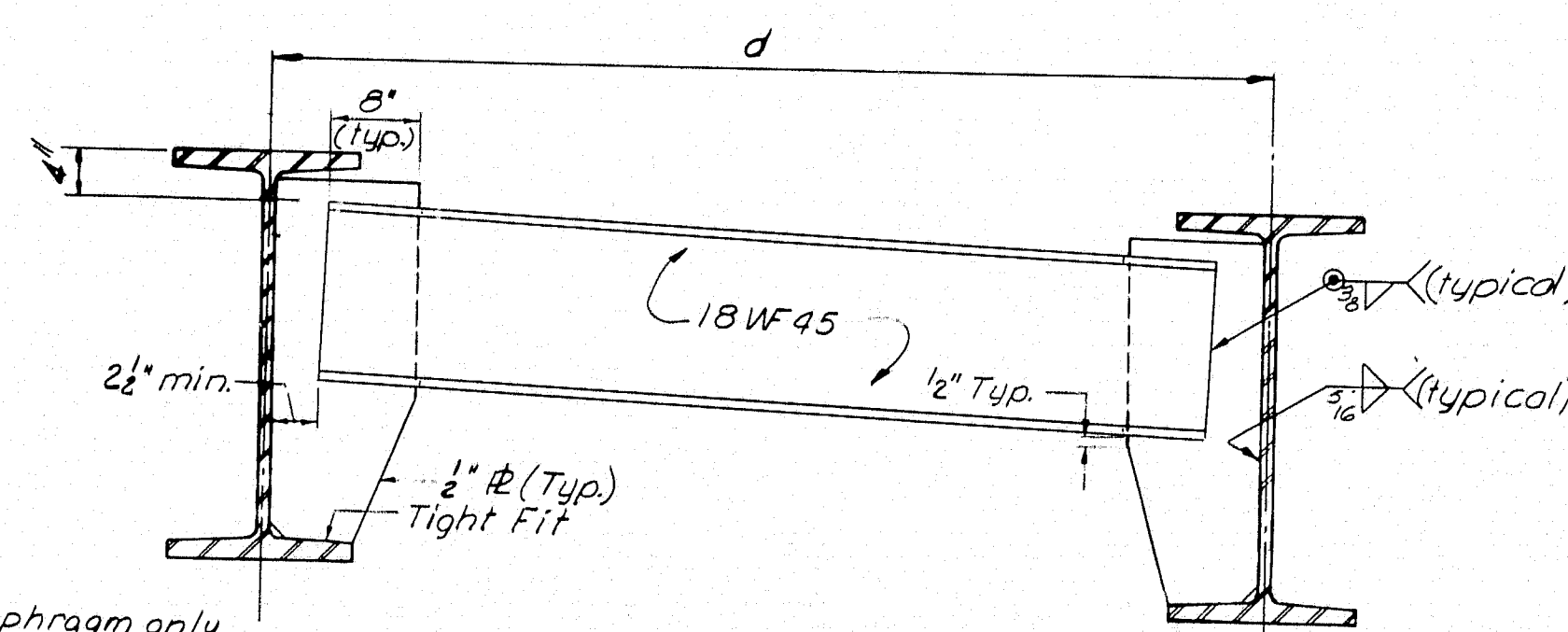
NOTE To compensate for dead load deflections as well as possible irregularities in beams, set elevations of points A & B bottom of slab elevations, at the points indicated before any slab formwork is started.



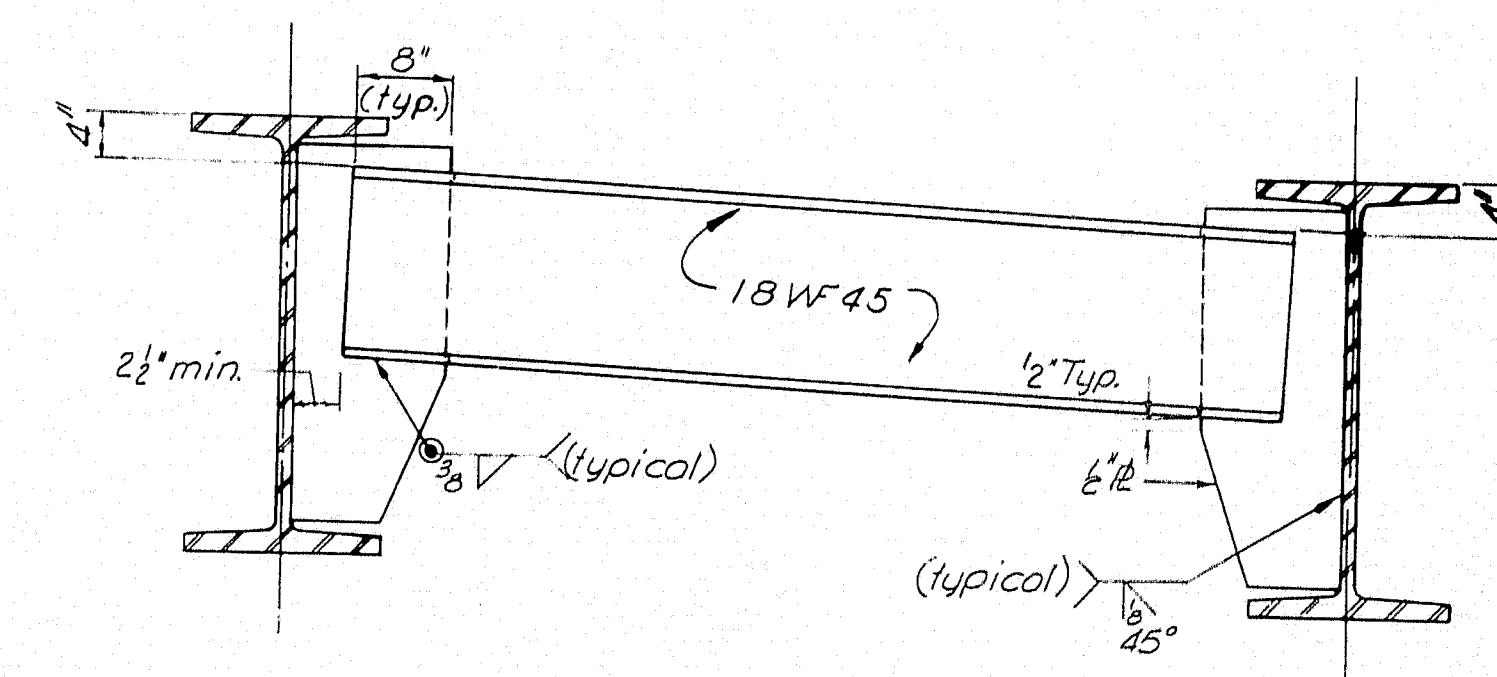
SHEAR CONNECTOR SPACING



TYPE A-I DIAPHRAGM (typical intermediate)
TYPE D-I DIAPHRAGM (at piers)
3' x 1'0"



TYPE B-DIAPHRAGM (at slab joints)
 $\frac{3}{4}'' - 1'-0''$



TYPE C DIAPHRAGM (at abutment)
 $\frac{3}{4}'' = 1'-0''$

NOTE:
For location of diaphragms,
see sheet 11 & 12.

HOWARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS

NEW YORK BOSTON KANSAS CITY

DESIGN— I.S.
TRACE—
CHECK— P.R.N.

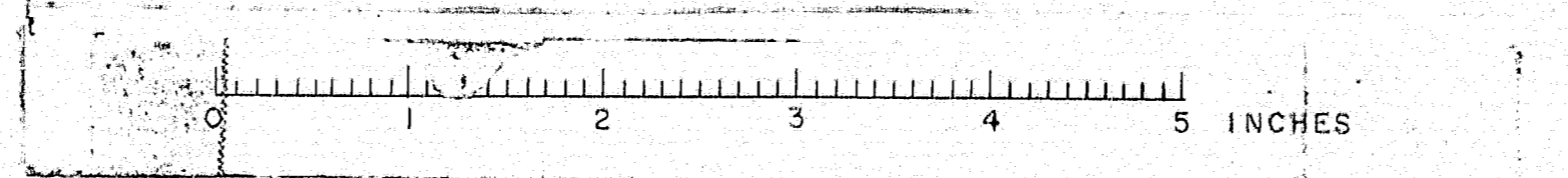
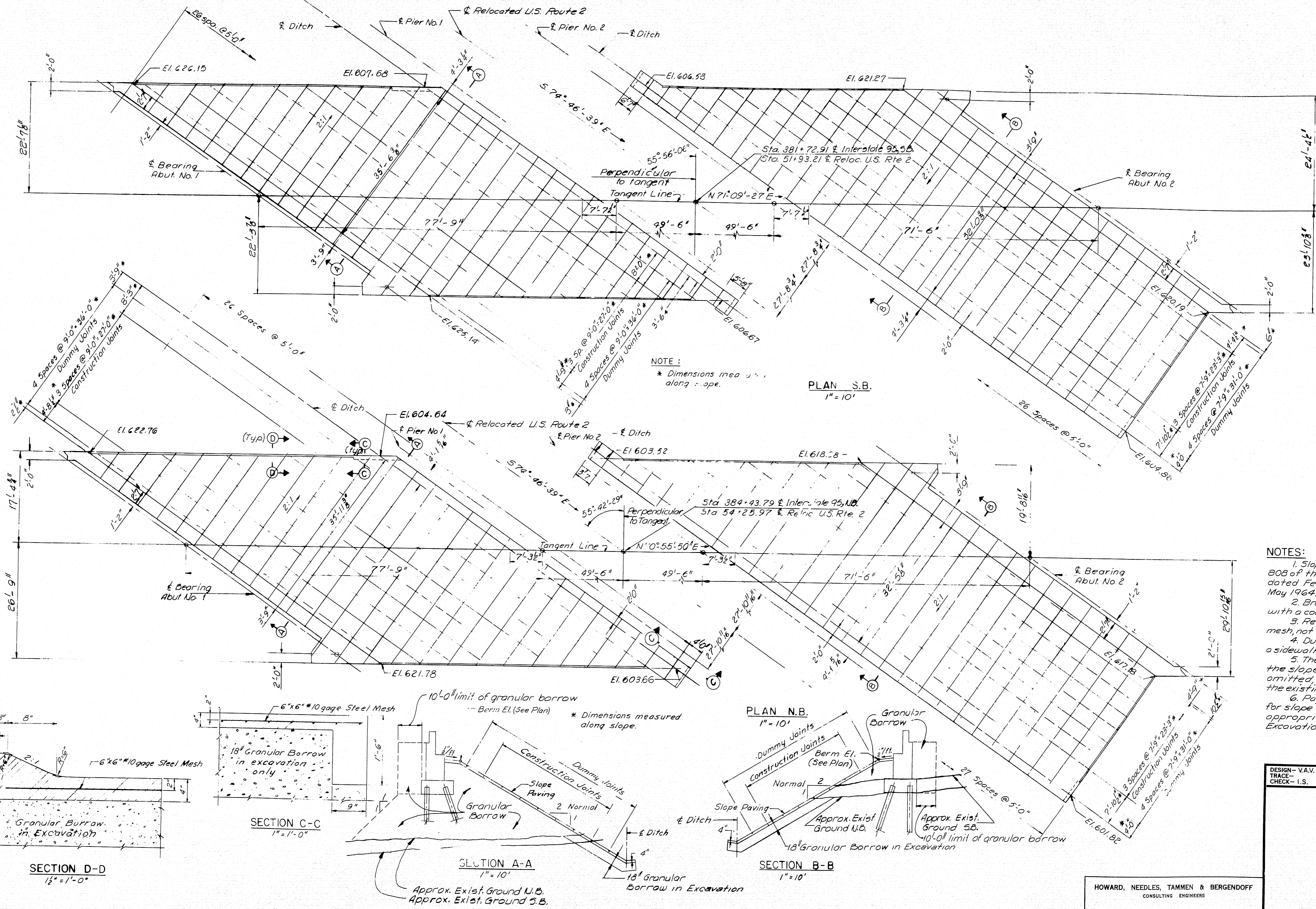
STATE HIGHWAY COMMISSION
BRIDGE DIVISION

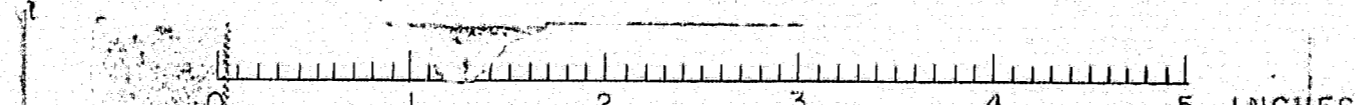
INTERSTATE 95 S.B.
OVER
U.S. ROUTE 2
IN THE TOWN OF
SMYRNA
AROSTOOK COUNTY
SUPERSTRUCTURE DETAILS

SHEET 15 OF 18 AUGUSTA, MAINE MARCH 1965

M-2237 SMYRNA (14)

M-2237 SMYRNA (14)





SPECIFICATIONS
DESIGN. A.A.S.H.T.O. Standard Specifications for Highway
Bridges, 1977 and Interim Specifications 1978.
CONTRACT. State of Maine, State Highway Commission,
Standard Specifications, Highways and Bridges,
Revision of June, 1968.

MATERIALS
STRUCTURAL STEEL. A.S.T.M. A36

BASIC ALLOWABLE STRESS
STRUCTURAL STEEL. $f_s = 20,000$ psi

NOTE
All work contemplated under this contract shall be governed by and in conformity
with the Standard Specifications (Revision of 1968) and supplements thereto,
except as modified on the plans and in the Special Provisions.

ESTIMATED QUANTITIES					
ITEM No.	DESCRIPTION	UNIT	QUANTITIES		
			I-95-9(73)8 I-95 S.B. over B&A RR (Oakfield)	I-95-9(74)8 I-95 over U.S. Route 2 (Smyrna)	I-95-9(73)8 I-95-9(74) (Combined)
202.20	Removing Bituminous Concrete Pavement	S.Y.	995	1,835	2,830
202.201	Removing Bit. Pave. (inc. Mem. Waterproofing)	S.Y.	1,325	2,170	3,495
403.09	Hot Bit. Pavement, Gra. C.-Gr. Ledge, Ton	Ton	285	495	780
403.121	Hot Bit. Pavement Grading & Shimming	Ton	28	51	79
504.7801	Armored Jt. Modification (Oakfield)	L.S.	1	-	1
504.7802	Armored Jt. Modification (Smyrna)	L.S.	-	1	1
508.14	Membrane Waterproofing-Epoxy Penetration Sealer	S.Y.	1,325	2,170	3,495
629.05	Labor, Straight Time	M.H.	30	30	60
630.06	Traffic Officers	M.H.	320	320	640
631.10	Air Compressor (including operator)	Hour	10	10	20
631.11	Air Tool (including operator)	Hour	10	10	20
631.171	Truck - small (including operator)	Hour	10	10	20
631.22	Front End Loader (including operator)	Hour	10	10	20
632.08	Warning Lights	Grp.	1	2	3
639.10	Field Office, Type C	Each	0.5	0.5	1
645.61	4" Solid White Pavement Marking Line	L.F.	1,500	3,000	4,500
645.62	4" Broken White Pavement Marking Line	L.F.	2,200	4,400	6,600
645.63	4" Solid Yellow Pavement Marking Line	L.F.	1,500	3,000	4,500
645.66	Removing 4 in. Pavement Marking Line	L.F.	3,800	7,600	11,400
652.23	Flashing Arrow Board	Each	1	1	2
652.31	Type I Barricades	Each	20	40	60
652.33	Drums	Each	10	20	30
652.35	Construction Sign	Sq. Ft.	150	316	466
652.36	Maintenance of Traffic Control Sign	Sq. Ft.	12	17	25
659.10	Mobilization	L.S.	Nec.	-	Nec.

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION



BUREAU OF HIGHWAYS
REHABILITATION
OF BRIDGE DECK
ON
I-95 OVER U.S. ROUTE 2
IN THE TOWN OF
SMYRNA
AROOSTOOK COUNTY
PROJECT NUMBER TQI-FI-95-91741284
LENGTH OF PROJECT 0.049 MILES
AND
I-95 OVER B & A RR YARD
IN THE TOWN OF
OAKFIELD
AROOSTOOK COUNTY
PROJECT NUMBER TQIG-FIG-95-91731279
LENGTH OF PROJECT 0.053 MILES

I-95 SB OVER U.S. ROUTE 2
OR
I-95 NB OVER U.S. ROUTE 2
TRAFFIC DATA
A.D.T. 1980 1835
A.D.T. 2000 2755
D.H.V. 394
T.(%) 21
D.(%) 100
V. 70 mph
18 KIPS 602

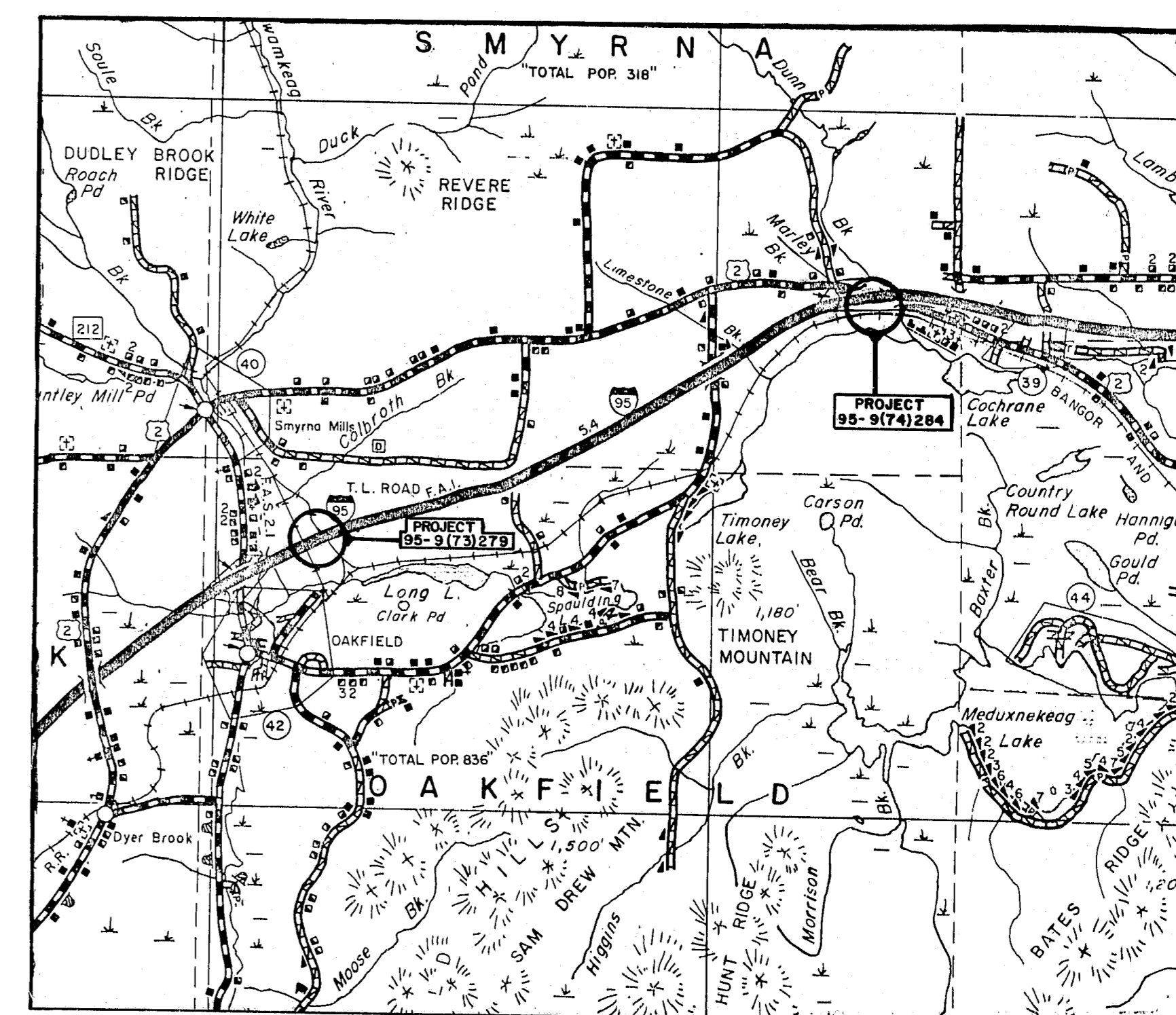
I-95 OVER B&A RR YARD
TRAFFIC DATA
A.D.T. 1980 1835
A.D.T. 2000 2755
D.H.V. 394
T.(%) 21
D.(%) 100
V. 70 mph
18 KIPS 602

INDEX OF SHEETS

1.	Title Sheet
2.	Rehabilitation Plan of Approaches
3.	Rehabilitation Deck Plan (Oakfield)
4.	Rehabilitation Deck Plan (Smyrna)

-STANDARDS-

FOR FIELD OFFICE AND CONSTRUCTION SIGNING
STANDARDS REFER TO PROJECT I-IG-95-9(101) STANDARDS.



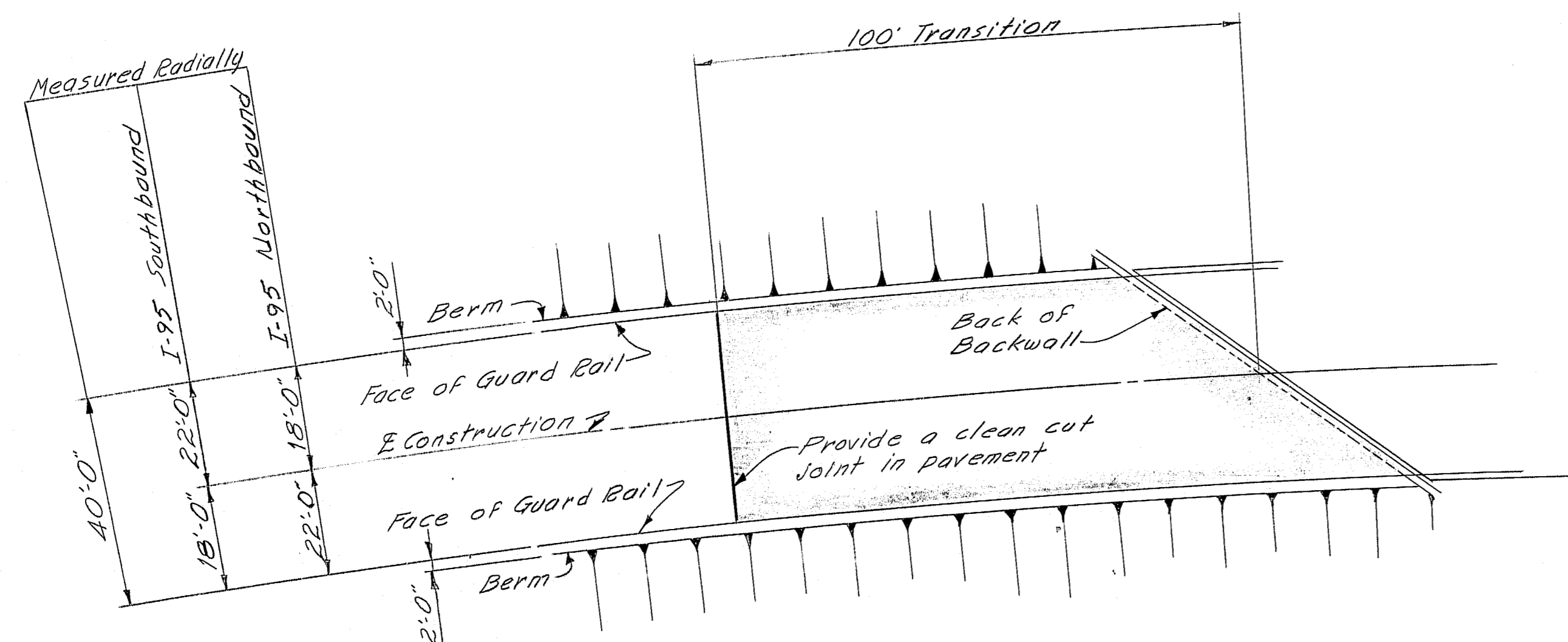
LOCATION MAP
1 0 1 2 3 4 MILES

APPROVED:
STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
COMMISSIONER
CHIEF ENGINEER & BUREAU DIRECTOR
DATE
8-11-77
8-11-77

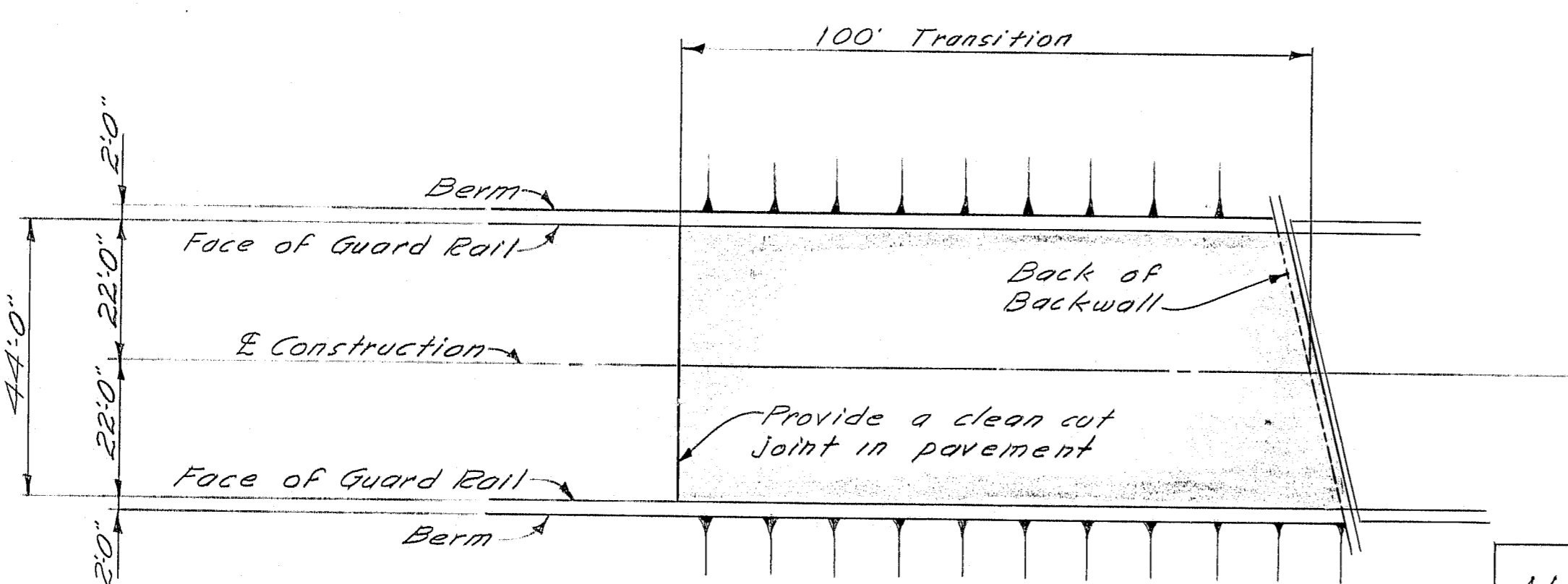
R92-475

As Built 1981
UNITED STATES
DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
REGION I
APPROVED:
DIVISION ADMINISTRATOR
DATE

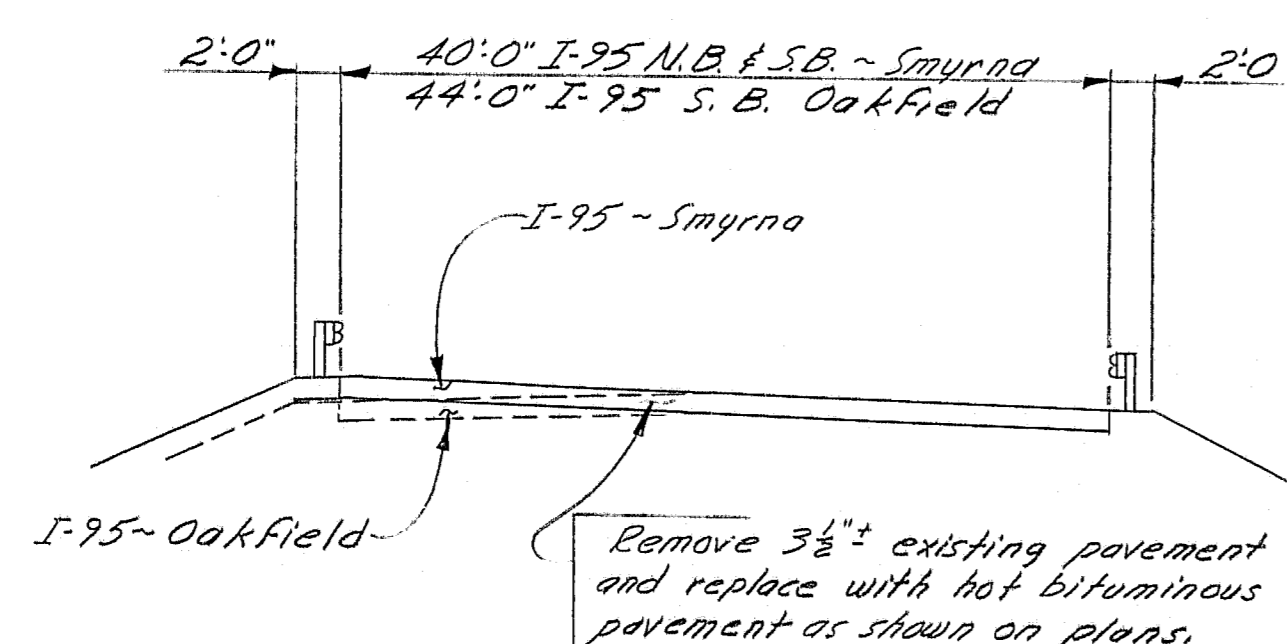
PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAILED	W. Lynde P.T.A.	3-79
CHECKED	PLM	3-79
REVISIONS		
FIELD CHANGES		
PLANS		



TYPICAL APPROACH PLAN
I-95 NB & SB SMYRNA



TYPICAL APPROACH PLAN
I-95 SB OAKFIELD



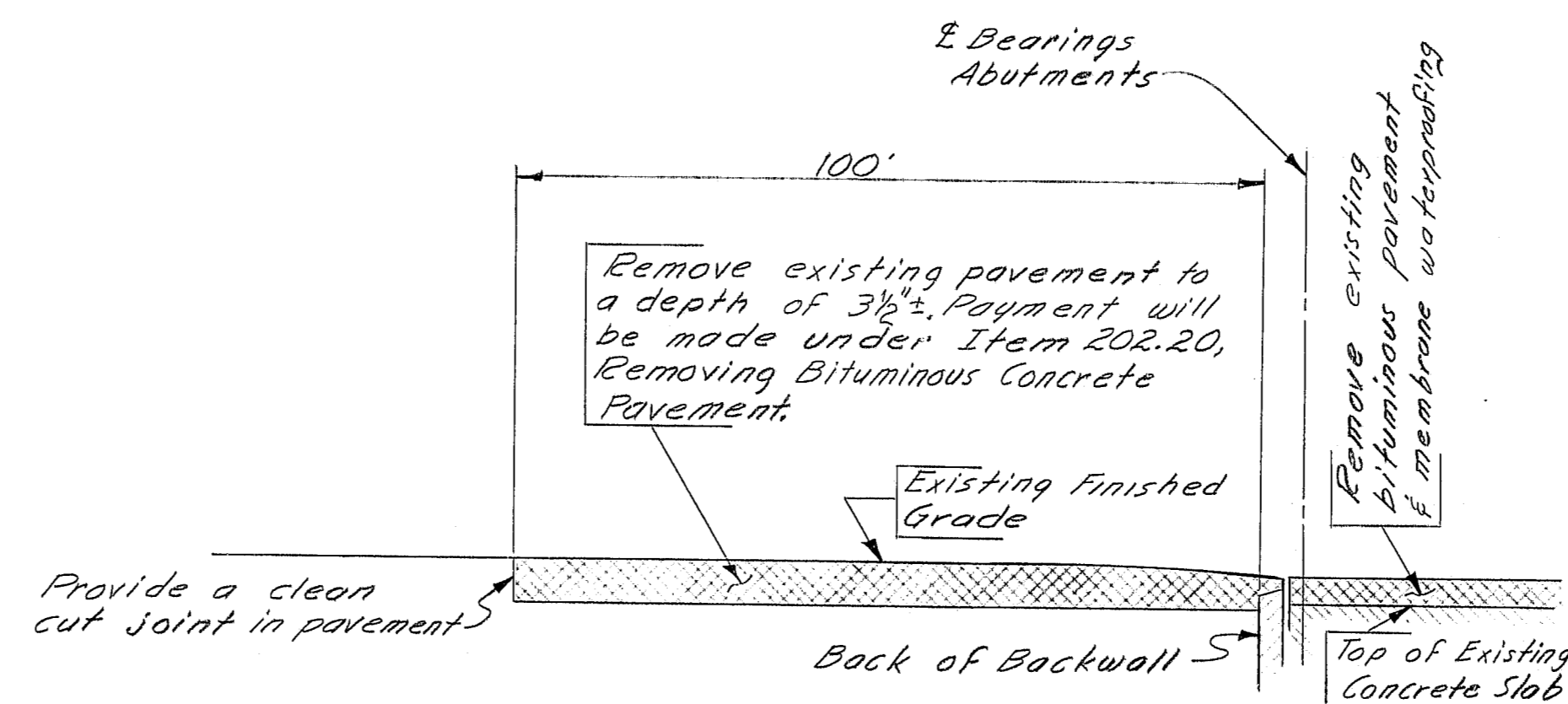
TYPICAL APPROACH SECTION
(Looking up-station)

NOTES - SMYRNA

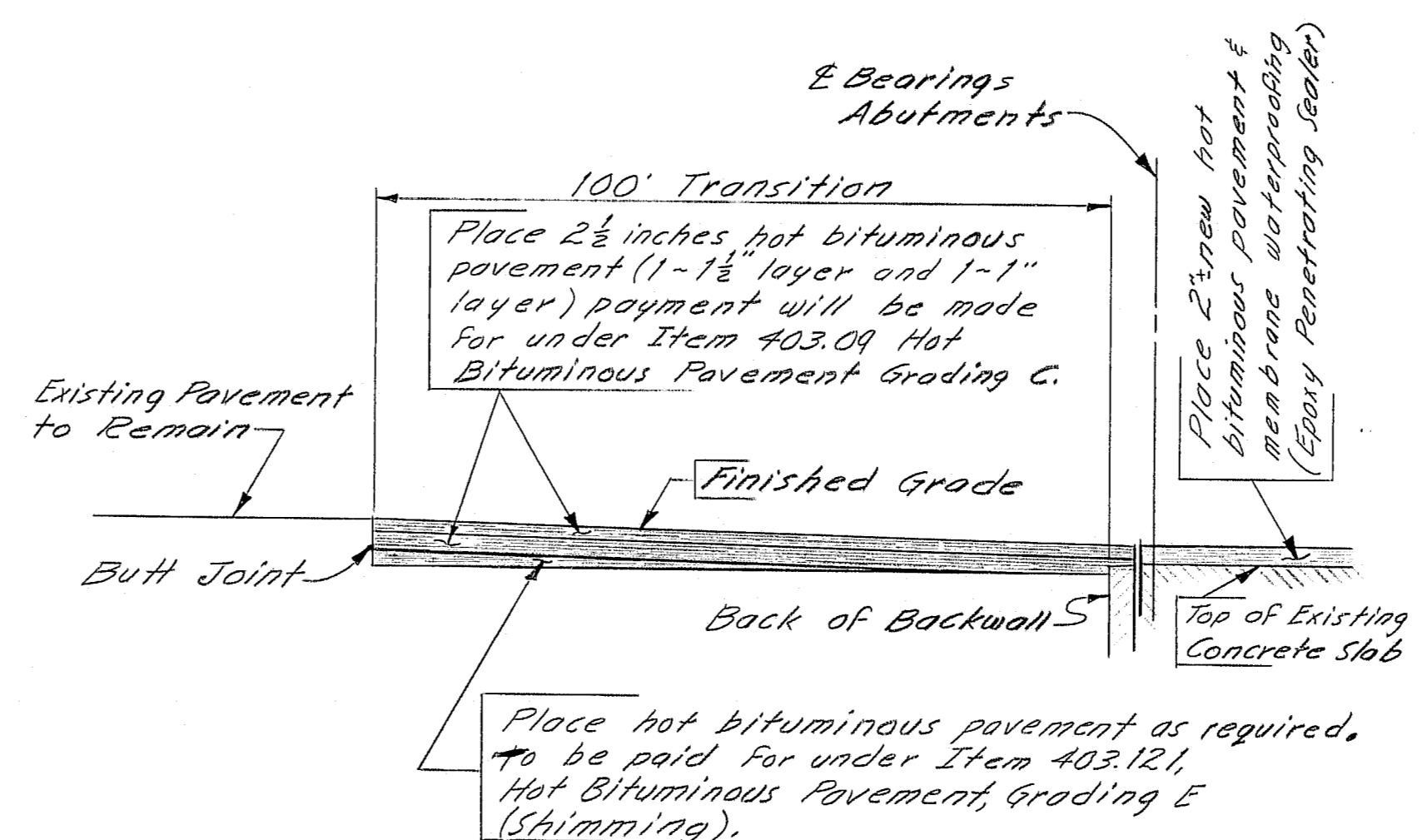
- The Existing concrete shall be removed from the superstructure slab at the armored joints as shown on the plans. Payment for removing concrete as described will be incidental to Item 504.7802, "Armored Joint Modifications, Smyrna."
- Preformed Elastic Joint Seals are not required for the curb joints.
- The Preformed Elastic Joint Seals shall have a minimum Movement Rating of 1.00 inch.
- The joint openings will vary depending on the dimensions of the Preformed Elastic Joint Seals selected by the contractor. The joint opening shall be set according to the opening shown on the approved "Armored Joint" shop detail drawings.
- The Preformed Elastic Joint Seal shall be approved by the Engineer prior to fabrication of the armored joint. Payment for Fabrication, Delivery and Setting of Preformed Elastic Joint Seal will be incidental to contract items.

NOTES - OAKFIELD

- The Preformed Elastic Joint Seals to be furnished shall have a minimum Movement Rating of:
 - Abutment No. 1 = 0.625 inches
 - Pier No. 1 = 0.500 inches
 - Abutment No. 2 = 0.625 inches
- In addition to the minimum Movement Rating requirements given above, the Preformed Elastic Joint Seals furnished must also fit the existing joint openings. The existing joint openings are as follows:
 - Abutment No. 1 = 1.25 inches @ 45° R
 - Pier No. 1 = 1.5 to 2.0 inches (Movement of joint is due to live load deflection)
 - Abutment No. 2 = 1.25 inches @ 45° R
- At the curbs, the contractor shall cut and bend the Preformed Elastic Joint Seal as shown on the plan and as directed by the Engineer.
- The Preformed Elastic Joint Seal shall be approved by the Engineer prior to installation. Payment for Fabrication, Delivery and Setting of Preformed Elastic Joint Seal will be incidental to contract items.



EXISTING PROFILE ALONG E I-95



NEW TRANSITION
PROFILE ALONG E I-95

NOTES - GENERAL

- The contractor shall maintain traffic at all times with a minimum lane width of 15 feet.
- The contractor shall remove the existing bituminous wearing surface and membrane waterproofing from the bridge structures.
- The contractor shall remove the existing joint sealer and filler from the curb joint openings to the satisfaction of the Engineer. Payment for removing the existing joint sealer and filler will be incidental to Items 504.7801 and 504.7802, "Armored Joint Modifications."
- The contractor shall straighten existing bent armored joint steel by an approved method where directed by the Engineer. Payment will be made under the appropriate labor & equipment rental items. Payment for rental of equipment not included as an item in this contract will be made on a force account basis as provided in sub-section 109.04 of the Standard Specifications.
- No paint is required on any new or existing structural steel.
- The contractor shall replace any concrete removed from the superstructure slab with epoxy mortar (Road Patch, Set #3, Colma - DUE or equal) as outlined in Special Provisions for Item 202.20, "Removing Bituminous Pavement (including membrane waterproofing)." Payment for epoxy mortar will be incidental to Item 504.7801 or Item 504.7802, "Armored Joint Modifications."
- At the armored joints, the contractor shall cut and grind smooth the existing armored joint steel as shown on the plans. The surface of both the new and existing steel that will be in contact with the Preformed Elastic Joint Seal will be cleaned to the satisfaction of the Engineer. Payment for cutting and grinding and cleaning will be incidental to Item 504.7801 or Item 504.7802, "Armored Joint Modifications."

REFERENCES

- For Rehabilitation Deck Plan Oakfield see sheet #3
- For Rehabilitation Deck Plan Smyrna see sheet #4

As Built in 1981

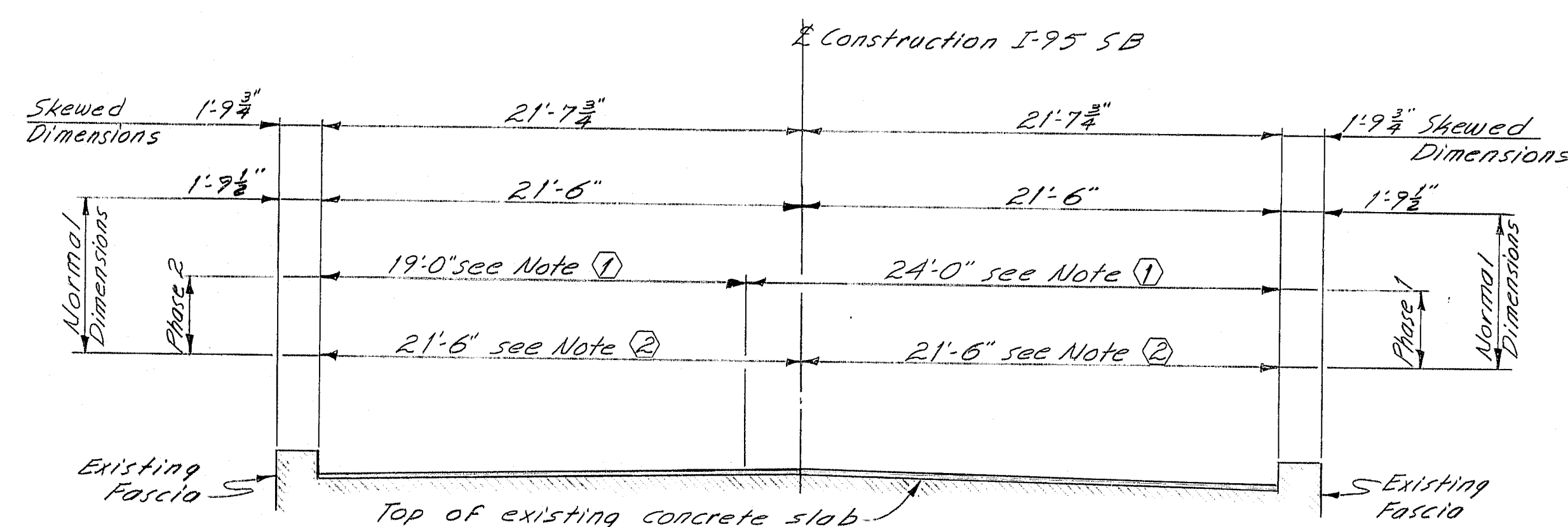
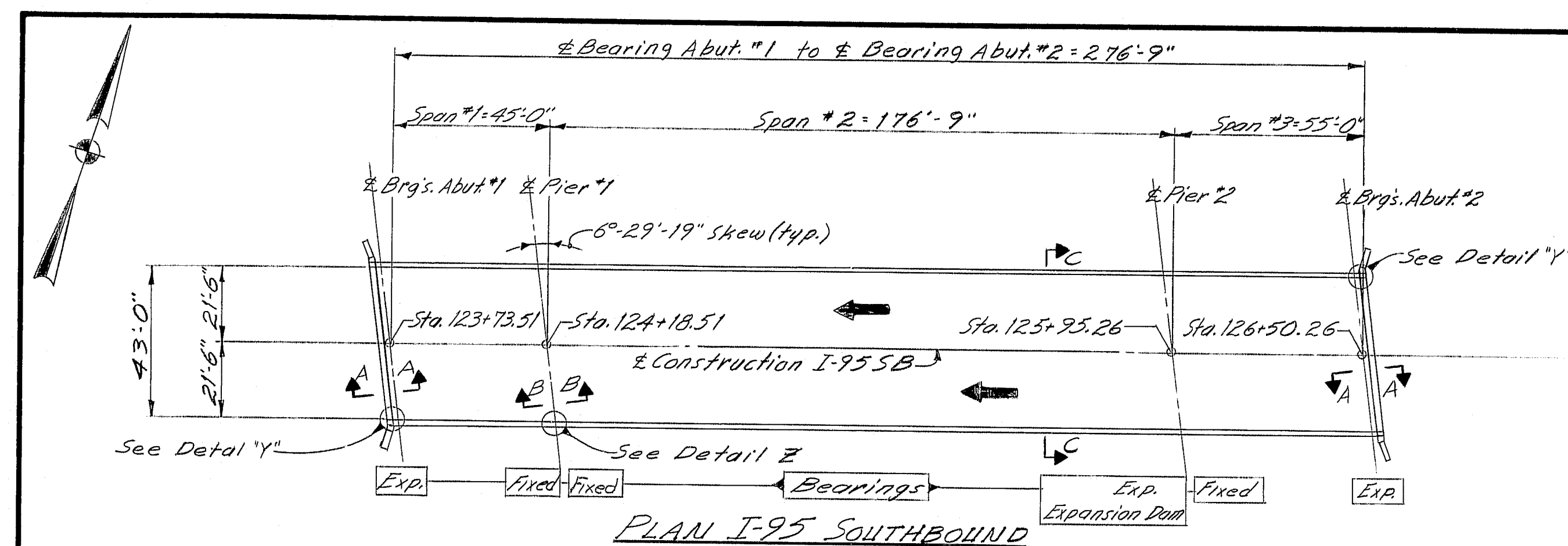
Plans of the existing bridge are available for the Contractor's reference at the Bridge Design Office in Augusta. The plans are reproductions of original drawings as prepared for the construction of the bridge and it is very unlikely that the plans will show any construction field changes or any alterations which may have been made to the bridge during its life span.

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

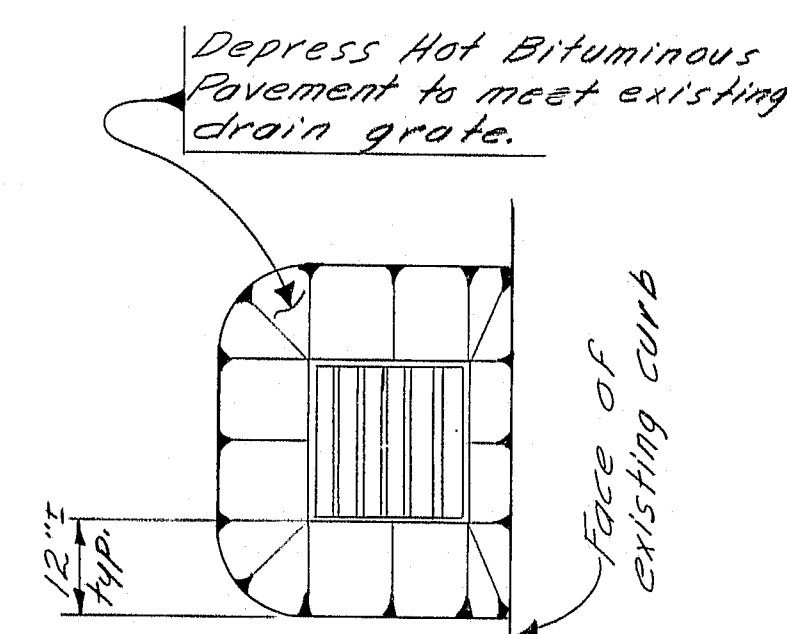
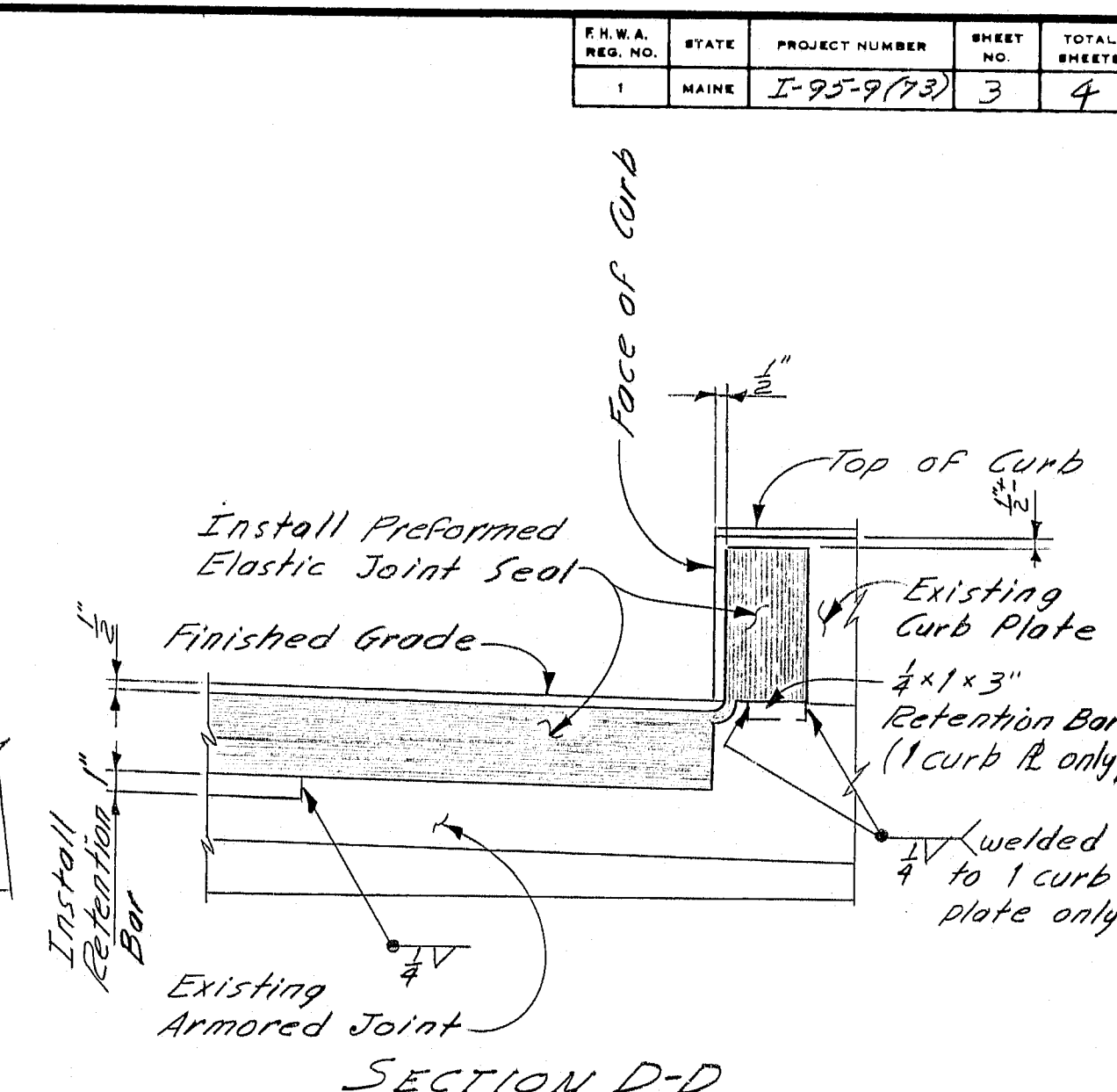
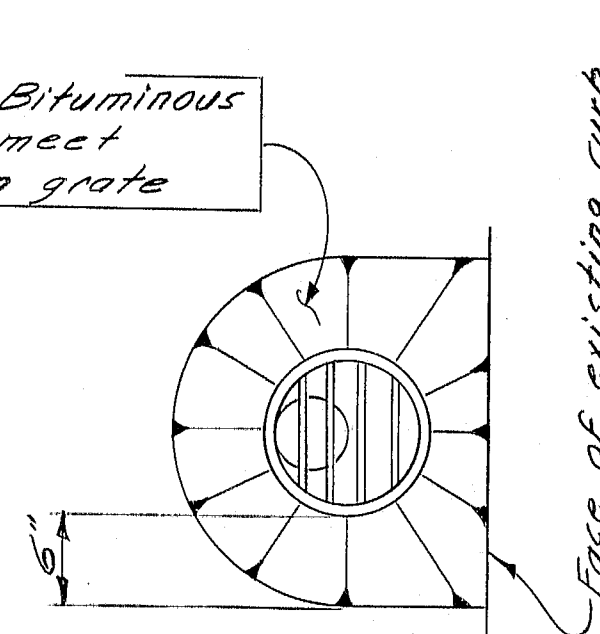
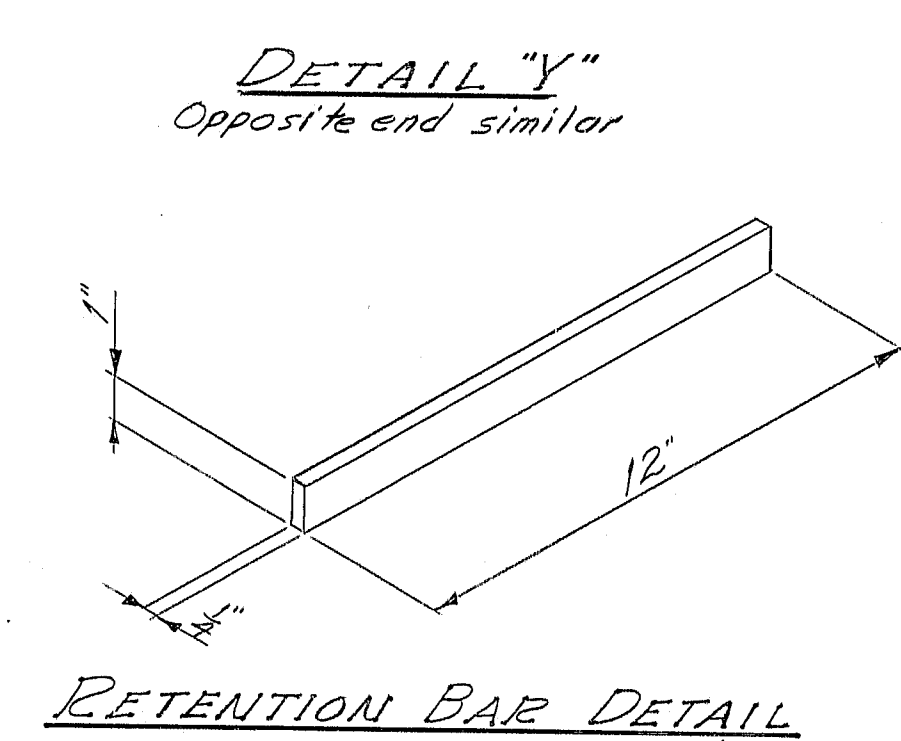
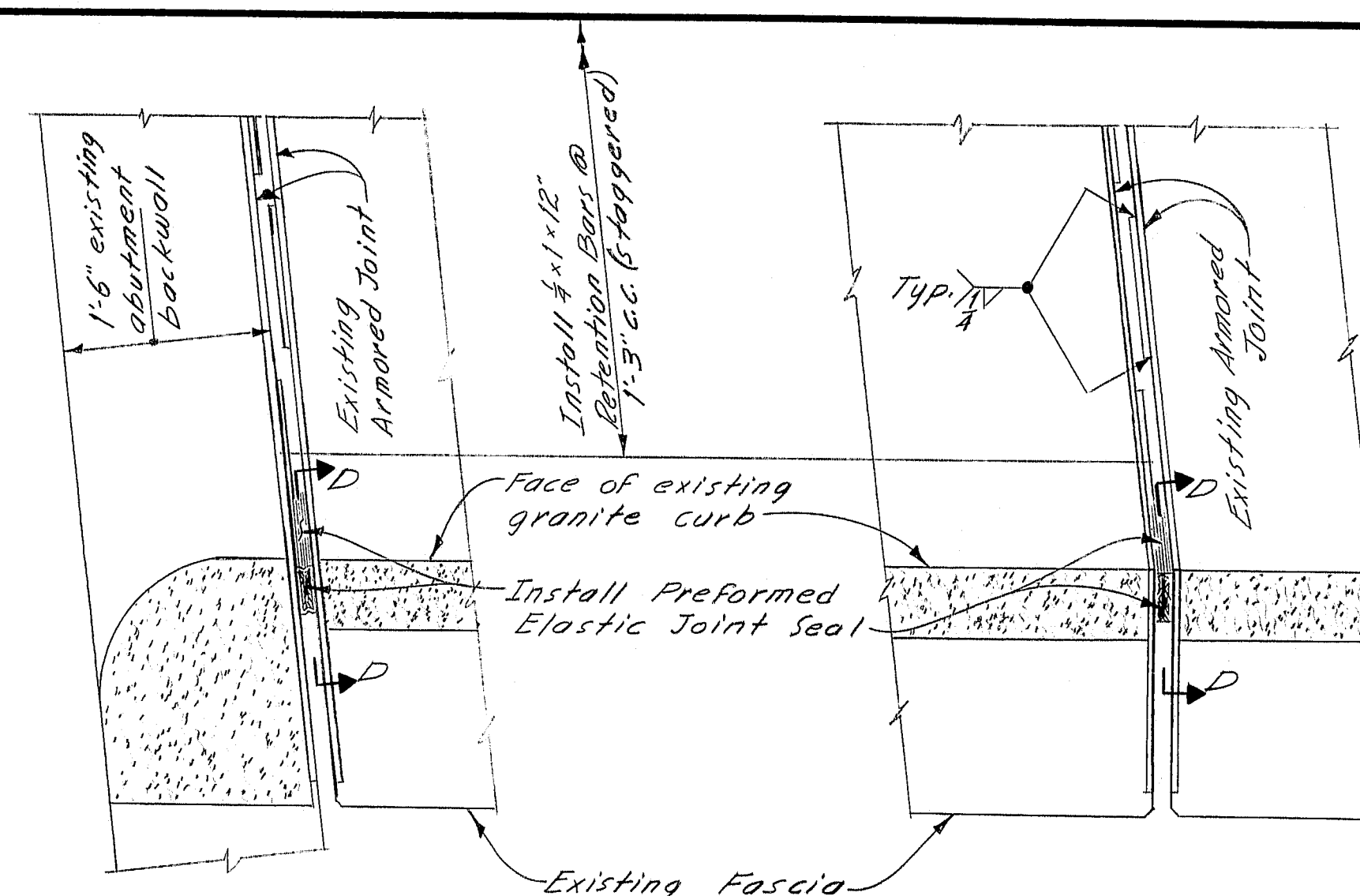
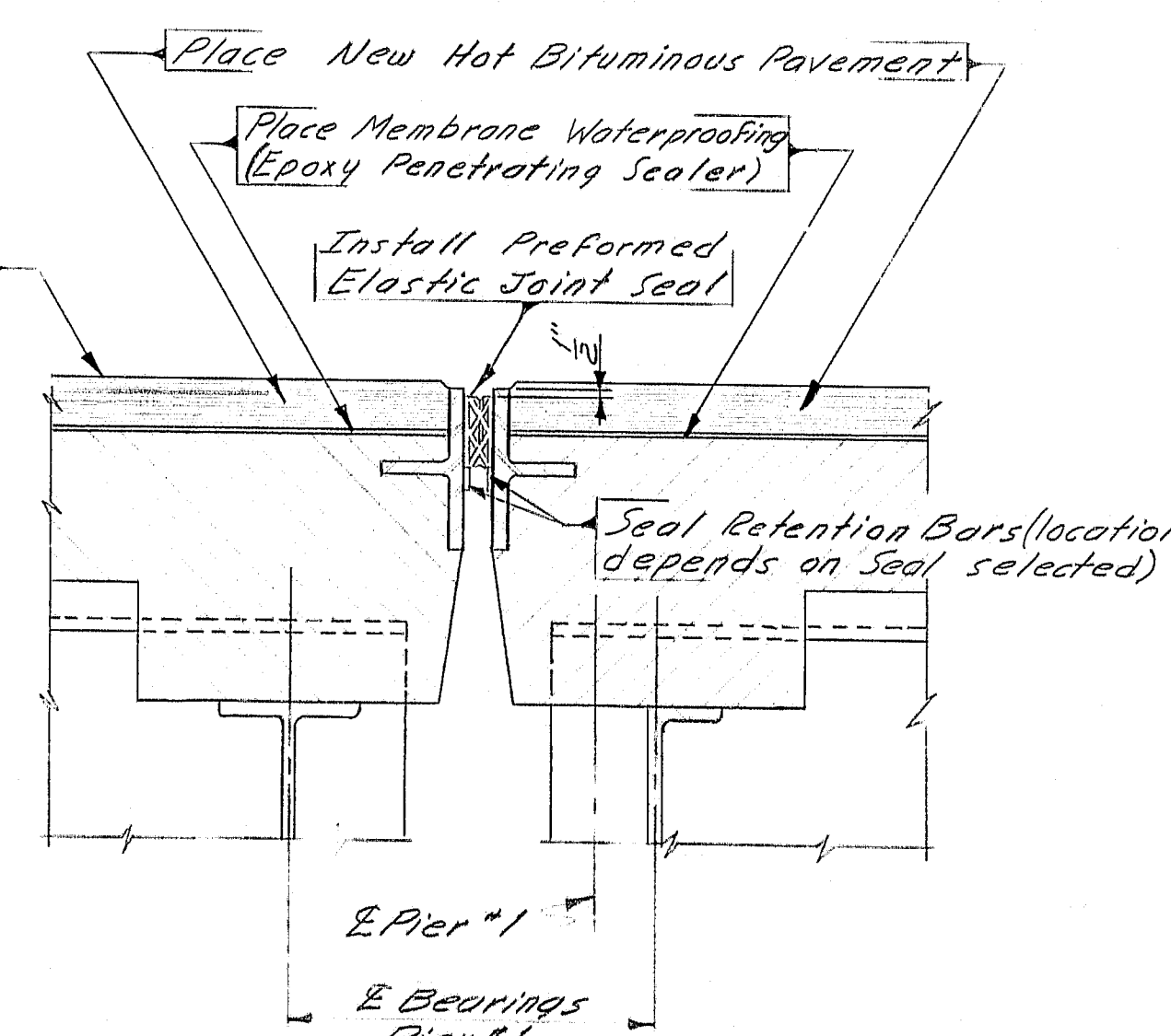
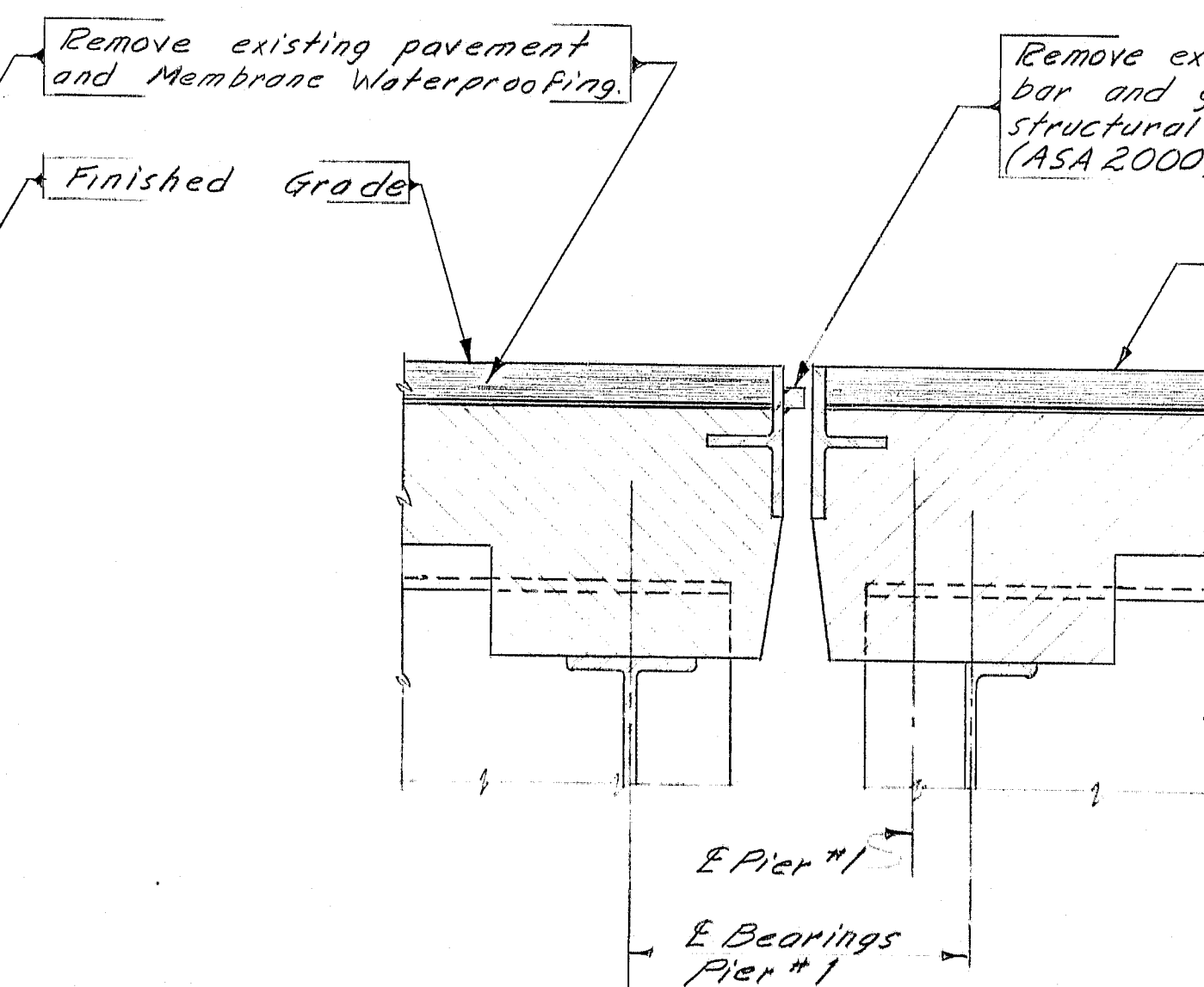
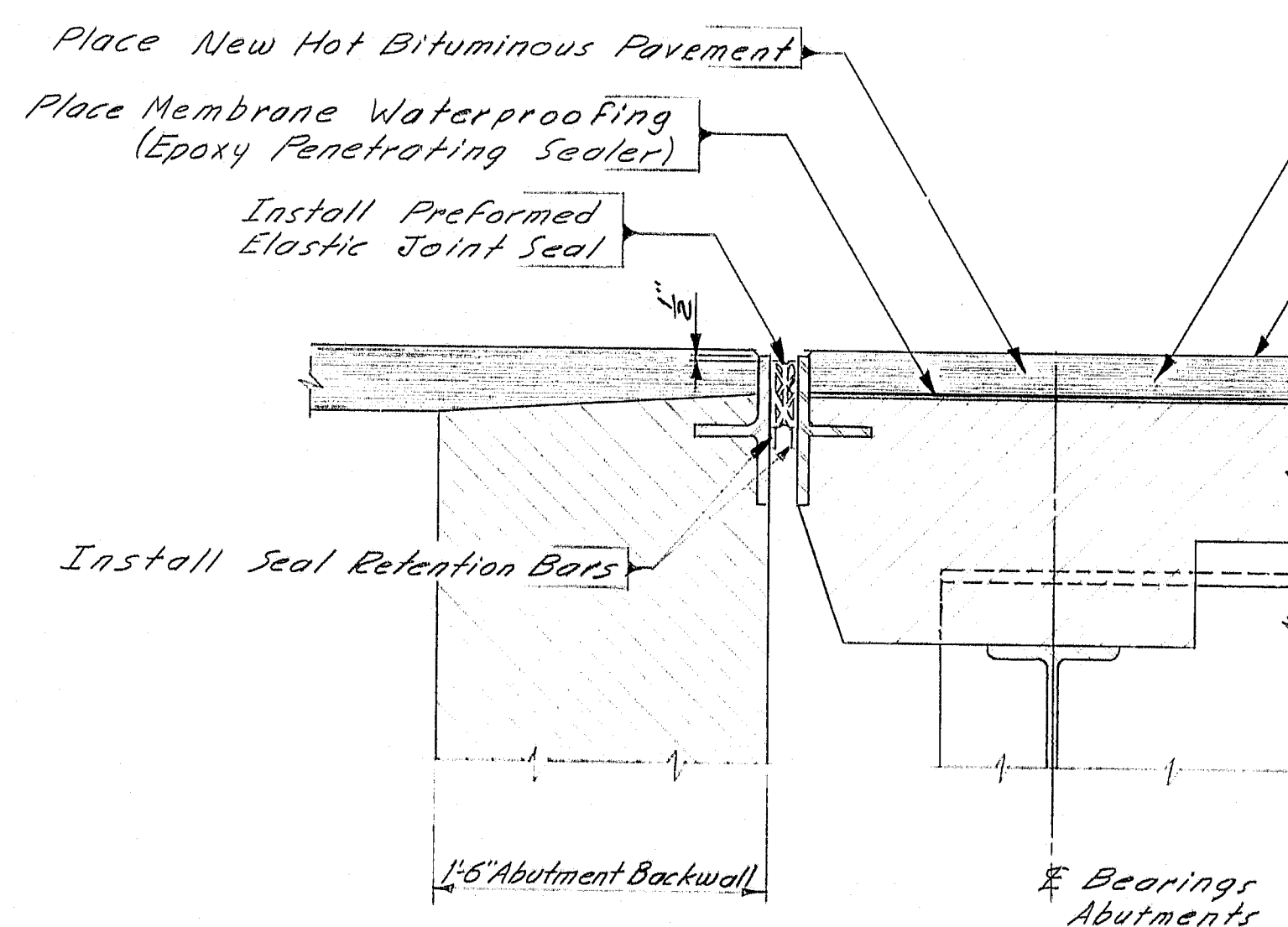
INTERSTATE-95
OVER
B & A RR YARD & U.S. ROUTE 2
IN THE TOWNS OF
OAKFIELD & SMYRNA
AROOSTOOK COUNTY
REHABILITATION PLAN OF APPROACHES
SHEET 2 OF 7 AUGUSTA, MAINE March 1980

R92-476

PROJECT DESIGN ENGINEER	BY	DATE
DESIGN - DETAILED	W. L. M. R. T. M.	3-77
CHECKED	R. B. M.	3-77
PLANS		
REVISIONS		
FILED CHANGES		



Note (1) Remove Existing Pavement & Membrane Waterproofing
Note (2) Place Membrane Waterproofing (Epoxy Penetrating Seal) and 2" Hot Bituminous Pavement



REFERENCES

For Notes see sheet #2

For Approach Plans & Details see sheet #2

NOTE

Work shall not begin on this structure until northbound traffic has been allowed on new adjacent northbound bridge.

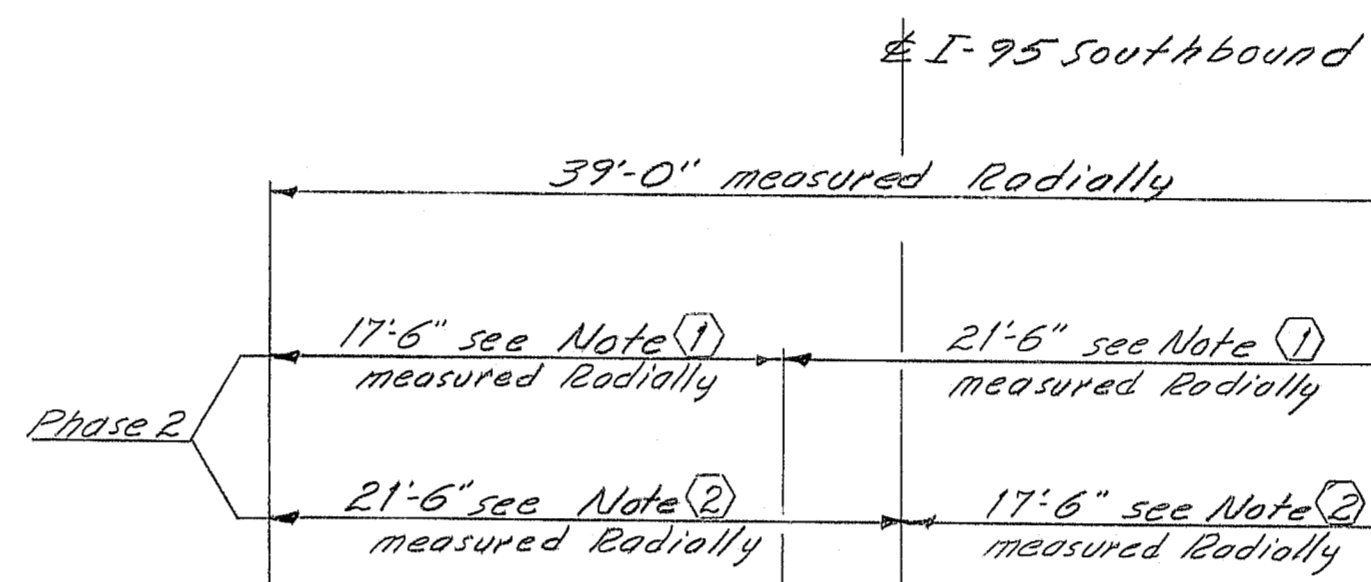
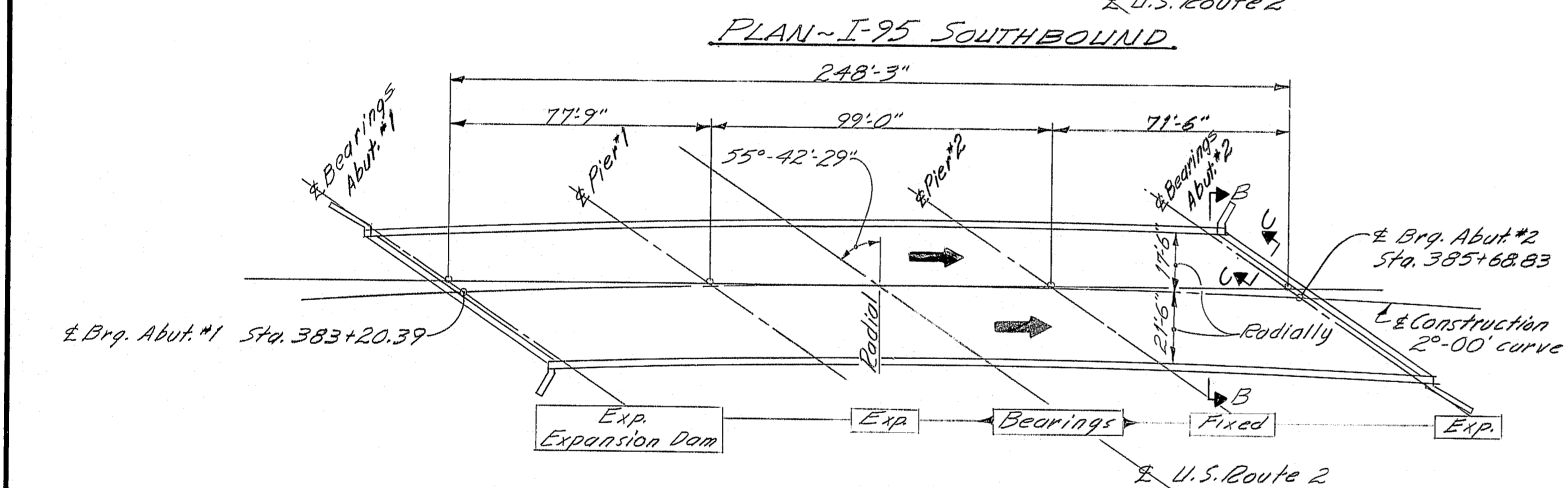
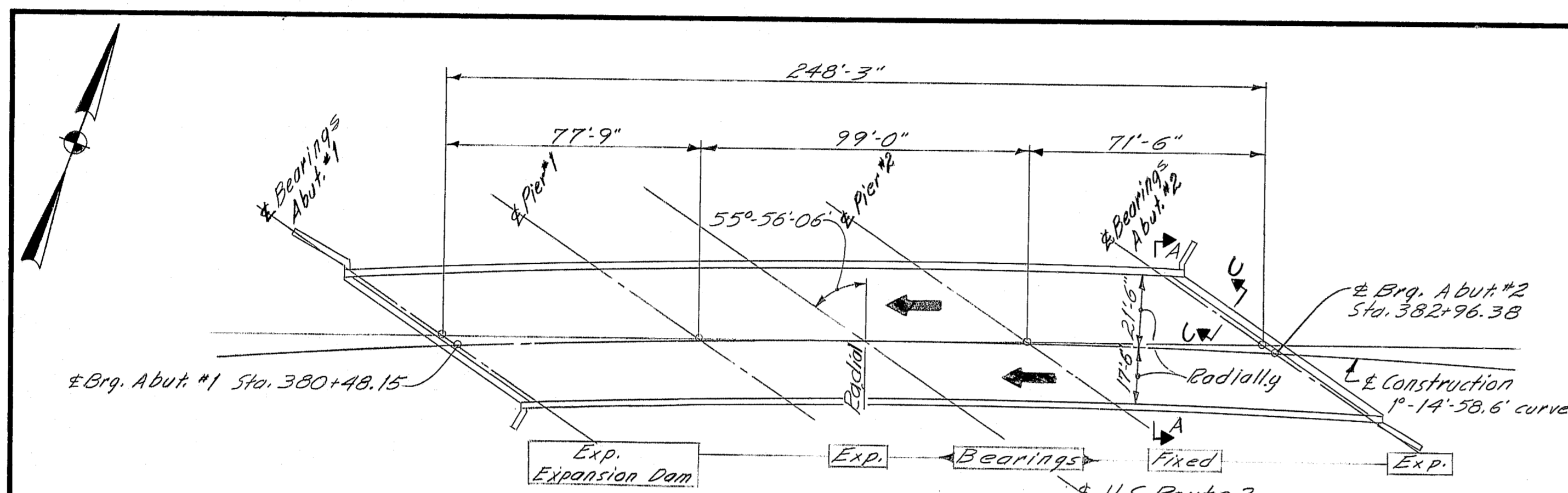
As Built 1981

STATE OF MAINE
DEPARTMENT OF TRANSPORTATION

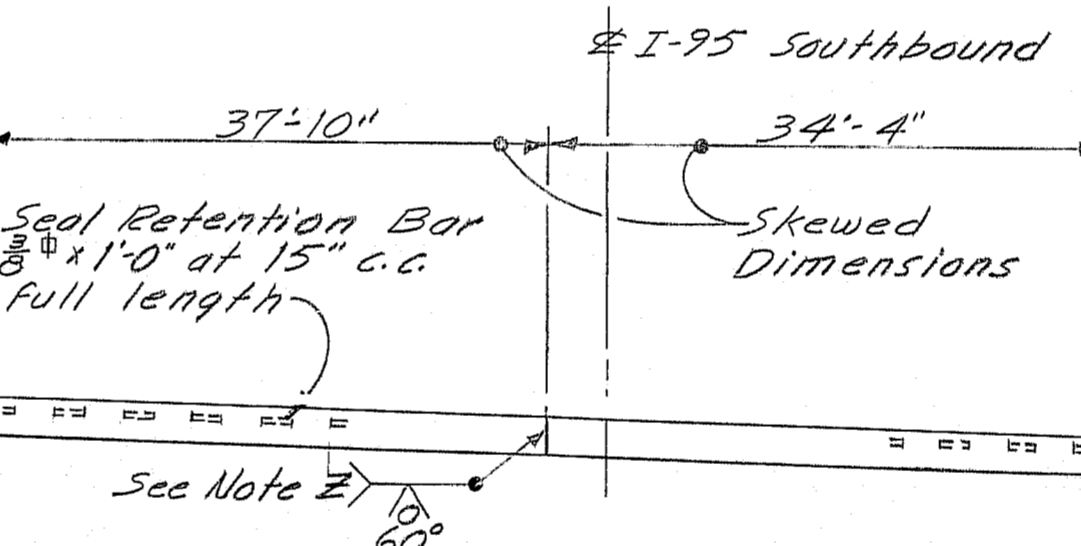
INTERSTATE-95
OVER
B & A RR YARD
IN THE TOWN OF
OAKFIELD
AROOSTOOK COUNTY
REHABILITATION DECK PLAN

R92-477

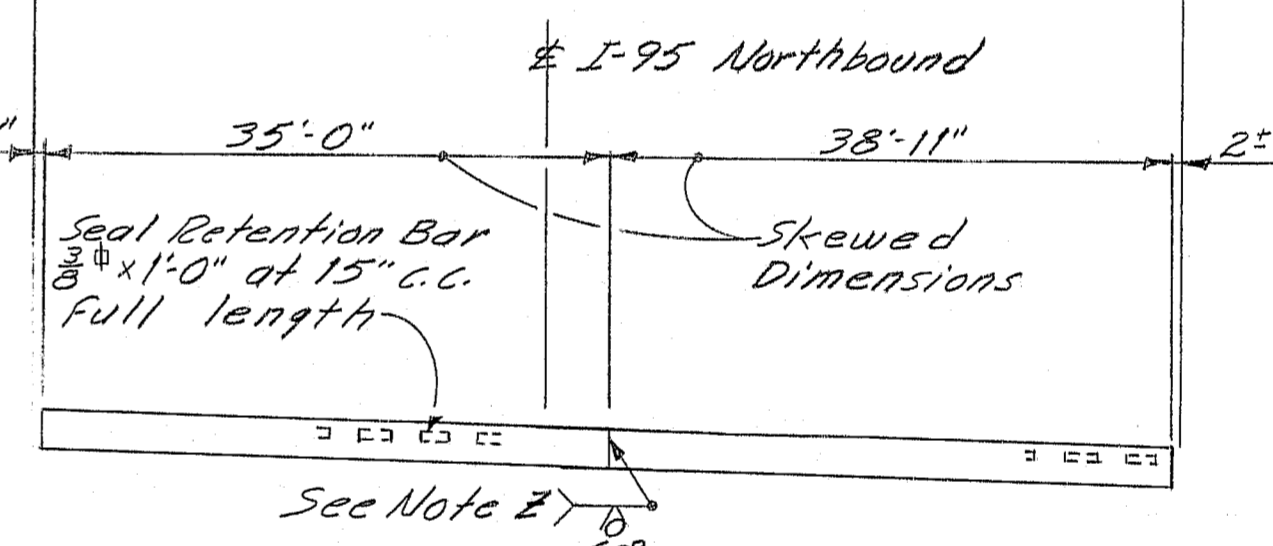
F.R.A. REV. NO.	STATE	PROJECT NUMBER	SHEET NO.	TOTAL SHEETS
1	MAINE	I-95-9 (74)	4	4



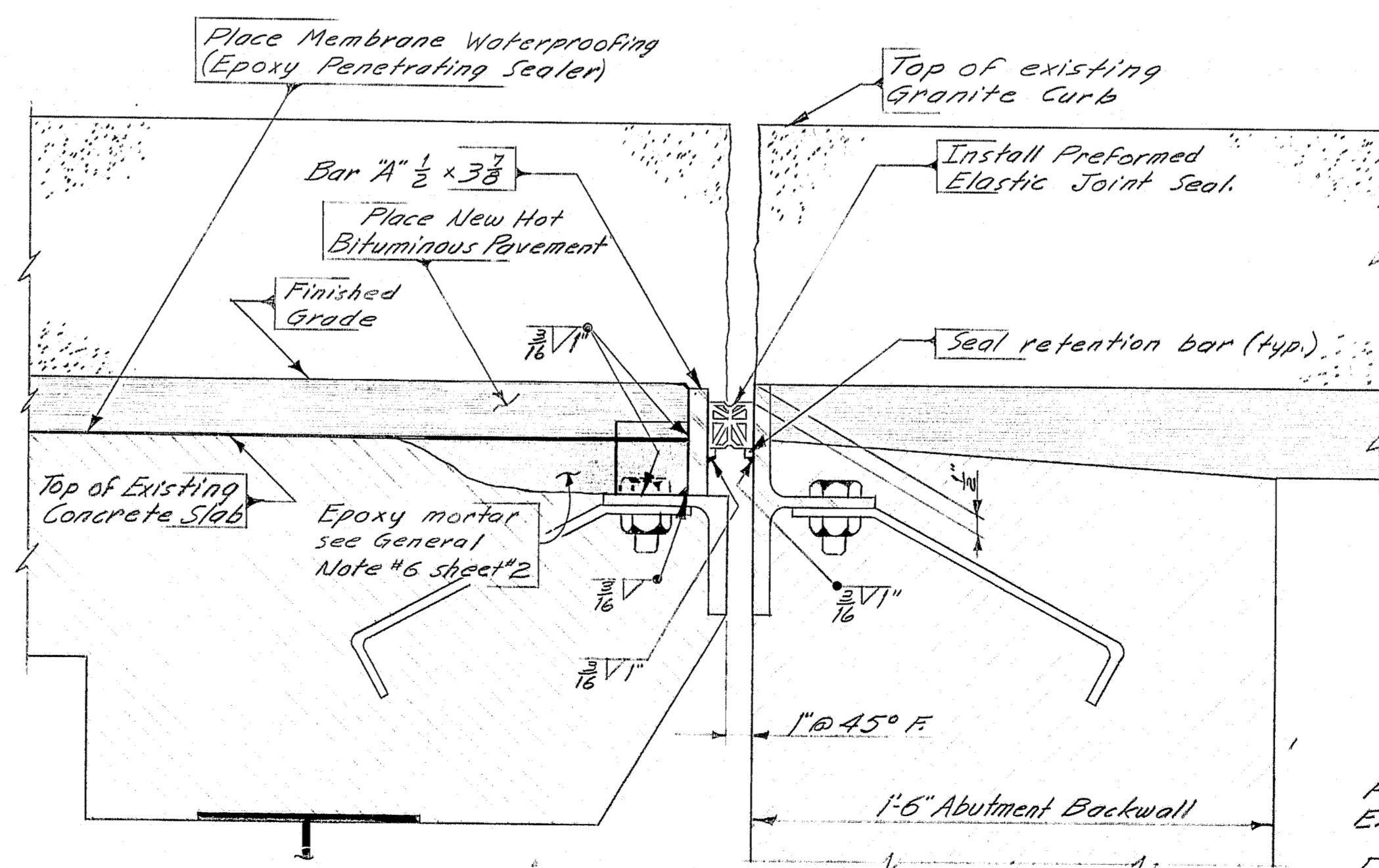
Note ① Remove Existing Pavement & Membrane Waterproofing
Note ② Place Membrane Waterproofing (Epoxy Penetrating Seal) and 2" Hot Bituminous Pavement



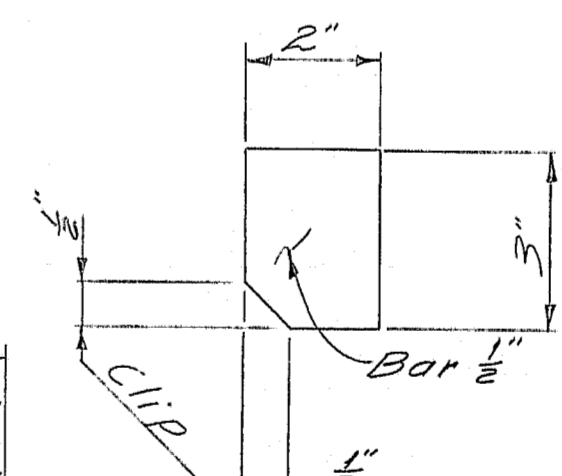
Note ① Remove Existing Pavement & Membrane Waterproofing
Note ② Place Membrane Waterproofing (Epoxy Penetrating Seal) and 2" Hot Bituminous Pavement



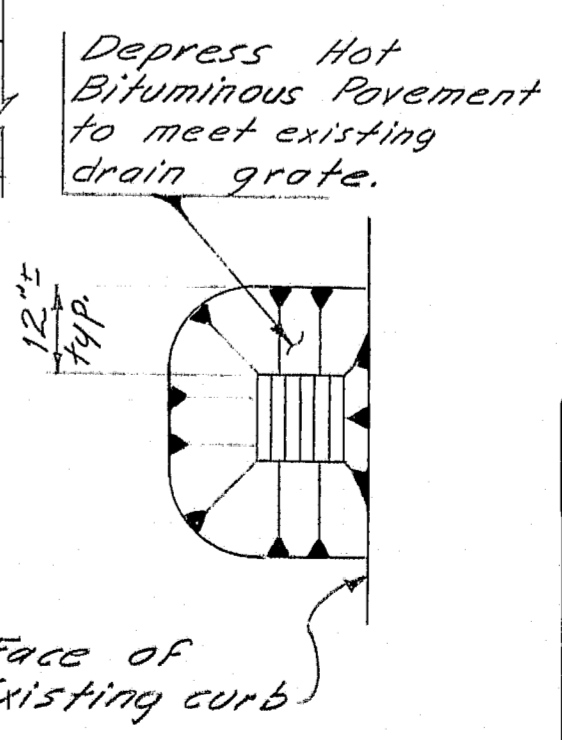
EXISTING ARMORED JOINT - SECTION C-C
Abutment #2 only



NEW ARMORED JOINT - SECTION C-C
Abutment #2 only



BAR B' DETAIL
(Spaced at 2'-0")



DRAIN DETAIL

NOTE Z
Grind smooth on face where Preformed Elastic Joint Seal will be installed.

REFERENCES
For Notes see sheet #2
For Approach Plan and Details see sheet #2

As Built 1981
STATE OF MAINE
DEPARTMENT OF TRANSPORTATION
INTERSTATE-95
OVER
U. S. ROUTE 2
IN THE TOWN OF
SMYRNA
AROSTOOK COUNTY
REHABILITATION DECK PLAN

R92-478